



690 Chesterfield Pkwy W • Chesterfield MO 63017-0760 Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

### **Planning Commission Staff Report**

**Project type:** Amended Site Development Section Plan

Meeting Date: August 14, 2023

From: Shilpi Bharti, Planner

**Location:** 17365 Chesterfield Airport Road.

**Description:** Chesterfield Commons Six, Lot 3 (ASDSP): A Third Amended Site Development Section

Plan, Amended Landscape Plan, Lighting Plan and Amended Architectural Elevations for a 1.28 acre tract of land zoned "C-8" Planned Commercial District located on the north side

of Chesterfield Airport Road and west of Boone's Crossing.

### **PROPOSAL SUMMARY**

CPH, LLC has submitted an Amended Site Development Section Plan, Amended Landscape Plan, Lighting

Plan, and Amended Architectural Elevations for an existing fast-food restaurant Chick-fil-A located at 17365 Chesterfield Airport Road.

Proposed Development includes:

- Expanding of Drive-thru Lane
- Removing 14 parking spaces
- Parking reduction request
- Relocating existing order canopy
- New canopy proposed on the east
- Amended Landscape Plan
- Lighting Plan



Figure 1: Subject Location

### **HISTORY OF SUBJECT SITE**

The subject site is in the Chesterfield Commons Six subdivision. The subdivision is a 19.1-acre tract of land located north of Chesterfield Airport Road. The site was rezoned from "NU- Non-Urban District" to "C-8" Planned Commercial District" by St. Louis County in 1988 following the Ordinance 13,933. In 2004, the Ordinance was amended to allow drive-through restaurants on the outparcels along Chesterfield

Airport Road and an increase in the height of the light standards following the ordinance 2096. A Site Development Section Plan for Lot 3 was approved in 2006. A small amendment was submitted to the City which was approved in 2007 to make minor changes to the drive-thru lanes. In 2013, Amended Site Development Section Plan was approved to allow a 477 square foot addition, removal of outdoor seating area and addition of a dual drive-thru lane. An 8% of parking reduction was also approved with the second Amended Site Development Section Plan.

### SURROUNDING LAND USE AND ZONING

The subject site is zoned C8 - Planned Commercial. Property on north is zoned PI—Planned Industrial. Properties on west and south are zoned PC-Planned Commercial. Property east of the subject site is zoned C8 - Planned Commercial.



Figure 2: Zoning Map

### **STAFF ANALYSIS**

The existing site experiences undesirable congestion issues. Currently, the drive-thru customers for Chick-fil-A extends beyond the property line queueing onto Arnage Rd. (private street). In order to accommodate all the drive-thru customers inside the site and prevent conflicts between the drive-thru customers and customers seeking to park in the lot, the restaurant is expanding their existing two-lane drive-thru lane and eliminating 14 of the existing parking spaces from the site plan. The existing menu board and canopy on the south are proposed to be relocated to the east of the property.



Figure 3: Aerial of Site

Figure 4: Amended Site Plan

### **Elevations**

Applicant is amending the East Elevation to include a new canopy to service the drive thru. The existing canopy and menu board along Chesterfield Airport Road are proposed to be relocated to west side of the subject site.



Figure 5:East side Elevation

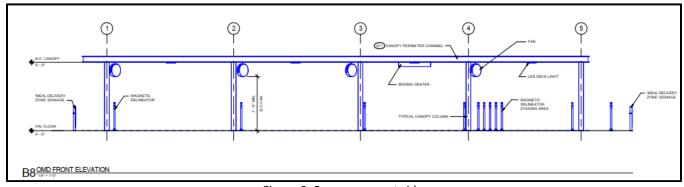


Figure 6: Canopy on west side

### **Parking**

As per City of Chesterfield Unified Development Code, minimum parking requirement for fast food restaurant is 15/1000 GFA. The minimum parking required for the existing 4,741 sq. ft. Chick-fil-A restaurant is 71 parking spaces. The site has previously been approved for a parking reduction. As the proposal further reduces parking on the site, an additional parking reduction is required. Parking reductions in excess of 20% require Planning Commission approval.

There is a shared parking agreement between Amini's and Outparcels 1, 2, 3 and 4. As per parking agreement, the outlots are permitted to utilize 21 shared parking spaces within the southern portion of the Amini's parking area. It should be noted that while Chick-fil-A (or any of the other outlots) has rights to share this parking, the parking cannot be used to satisfy the minimum parking requirements as they are not restricted as required by the UDC for that purpose. As a result, the proposal results in a 45% parking reduction being requested. The changes to the site will positively impact conflicts within the site and allows for better utilization of the parking spaces that will remain as there will be adequate room for drive thru queueing and entering/existing parking spaces. The attached parking reduction request notes that the, "dual drive-thru has been shown on average to decrease the number of dine in customers by 13%, decrease the number of carry out customers by 7%, and increase the number of drive-thru customers by 20%." While generally supportive of increasing the efficiency of the site, Staff would note concerns with further increasing volume to the drive-thru will further the congestion issues on Arnage Rd. and negatively impact adjacent properties.

In reviewing the parking demand study for the parking reduction, the UDC includes the following design features and review criteria.

- 1) The parking demand study provides sufficient number and types of spaces to serve the uses identified on the site.
- 2) Adequate provisions are made for the safety of all parking facility users, including motorists, bicyclists and pedestrians.
- 3) Sites are designed to minimize or alleviate traffic problems.
- 4) Parking spaces are located near the uses they are intended to serve and shall provide safe and convenient access for pedestrian access to the facility.
- 5) Adequate on-site parking is provided during each phase of development of the district.
- 6) The development provides opportunities for shared parking or for other reductions in trip generation through the adoption of transportation demand management (TDM) techniques to reduce trip generation, such as car pools, van pools, bicycles, employer transit subsidies, compressed work hours, and high-occupancy vehicle (HOV) parking preference.
- 7) Reductions in the number of parking spaces should be related to significant factors such as, but not limited to:
  - a) Shared parking opportunities;
  - b) Hours of operation;
  - c) The availability and incorporation of transit services and facilities;
  - d) Opportunities for reduced trip generation through pedestrian circulation between mixed uses;
  - e) Off-site traffic mitigation measures;

- f) Recognized variations in standards due to the scale of the facilities;
- g) Parking demand for a specified use; and
- h) The provisions of accessible parking spaces beyond those required per the UDC.

In particular, Staff would note concerns with satisfying requirement 3. to alleviate traffic congestion.

### Lighting

The site is updating the lighting fixture on existing light poles and proposing three new light poles on east, west and south. Applicant is also proposing a canopy light with maximum of 33.7fc. As per City of Chesterfield Unified development code, canopies light shall be adequate to facilitate the activities taking place as well as to provide a safe, secure environment.

### Landscaping

The Amended Landscape plan replace/relocate Six (6) existing trees, and include groundcovers and shrubs.

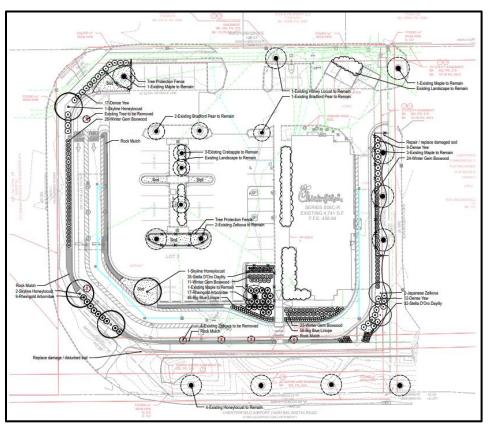


Figure 7: Proposed Landscaping

### **DEPARTMENTAL INPUT**

Staff has reviewed this proposed development and found it to be in compliance with the City's Unified Development Code and site-specific ordinances, with the exception of the minimum parking requirement.

All outstanding comments have been addressed at this time. Staff recommends Planning Commission to take action on the Amended Site Development Plan, Parking reduction request, Amended Landscape Plan, and Lighting Plan.

### **MOTION**

The following options are provided to the Planning Commission for consideration relative to this application:

- 1) "I move to approve (or deny) the Amended Site Development Section Plan, Amended Landscape Plan, Lighting Plan, Parking reduction, and Amended Architectural Elevations for Chesterfield Commons Six, Lot 3 as presented."
- 2) "I move to approve the Amended Site Development Section Plan, Amended Landscape Plan, Lighting Plan, Parking reduction, and Amended Architectural Elevations for Chesterfield Commons Six, Lot 3 with the following conditions..." (Conditions may be added, eliminated, altered or modified)

Attachments: Amended Site Development Section Plan Packet

Parking Reduction Request



Fax: 407.330.0639

July 14th, 2023

Parking Study For:

Chick-Fil-A; Store #1976 17365 Chesterfield Airport Road Chesterfield, MO 63005-1414

Chick-fil-A is planning to remodel the interior of their existing restaurant located at 17365 Chesterfield Airport Road in the City of Chesterfield. In conjunction with interior remodeling, Chick-fil-A plans to reconfigure the parking lot and drive aisle areas to significantly lengthen the second drive-thru lane as well as relocate the drive thru lane order point canopy in order to relieve traffic congestion during peak demand. As the site exists currently, the drive-thru layout does not adequately handle the current drive-thru customer volume, due to a short dual lane drive-thru configuration. The drive-thru lane currently backs up into the parking lot which negatively impacts both the drive-thru and dine in customers as parking spots are not accessible without waiting in line.

The proposed drive-thru improvements will have a positive impact on the site by allowing additional vehicle queuing, by increasing the speed at which customers can place their orders, and by reducing the percentage of carry out and dine in customers. The change will keep drive-thru traffic from backing up into the parking lot and increase the overall efficiency of drive-thru operations.

The proposed drive thru improvements require that the existing parking lot be reconfigured and will reduce available onsite parking from 53 to 39. Chick-fil-A has a cross parking agreement with the neighboring Amini's site which allows for the use of 21 parking spaces. The City of Chesterfield recognizes the use of twenty (21) of these spaces to satisfy the Chick-fil-A parking count. Therefore, with the additional offsite parking that the City recognizes, the proposed improvements will result in a total available parking count of 60 spaces to offer its customers for use at their restaurant.

Per Section 405.04.040 of the current City of Chesterfield Ordinance, parking spaces are to be provided at 15 spaces per 1,000 square feet of building area for the "Restaurant, fast-food" category. The existing building area is 4,741 square feet, and will stay the same after the proposed site improvements. Based on the current parking code, the building square footage will require Chick-fil-A to provide 71 parking spaces, which is eleven (11) more than what is proposed. Please see the proposed Site Development Plan under submission for detailed site layout information.

In previous phases of this sites development, the City of Chesterfield approved an 8% reduction of parking requirements due to the positive impacts of the Mulit-Lane Order Point (MLOP). Since a MLOP is still proposed at this time, Chick-fil-A would like to request an additional reduction for a total reduction of 15.5%. This would equate to a reduction of eleven (11) parking spaces, considering the required 71 parking spaces. In addition, based on typical store averages provided by Chick-fil-A, expanding an existing



dual lane drive-thru has been shown on average to decrease the number of dine in customers by 13%, decrease the number of carry out customers by 7%, and increase the number of drive-thru customers by 20%. The overall reduction of 20% of the number of customers that need to occupy parking spaces, which results in a decreased parking demand. Since Chick-fil-A is required by the City code of Ordinance to provide 71 parking spaces and since previous sites have shown a reduced parking demand of up to 20%. This parking analysis reflects a required parking count of 60 spaces, which is eleven (11) less than the City code typically requires with the proposed configuration. Please see the tables below which summarizes the Cities parking requirements, provided parking, and typical changes in drive-thru usage for Chick-fil-A sites that have significantly increased the length of an existing dual lane drive-thru.

City of Chesterfield Parking Requirements								
Use Requirement Units Required R								
Restaurant, Fast Food 15 spaces per 1,000 sq ft 4.741 71.1								
	15.5% Parking Reduction =							
				60				

Chick-fil-A Parking Summary									
Evicting Site	Building Sq Ft	Onsite Parking	Offsite Parking	Total Parking					
Existing Site	4,741	53	21	74					
Dramanad Cita	Building Sq Ft	Onsite Parking	Offsite Parking	Total Parking					
Proposed Site	4,741	39	21	60					

Summary of Expanded Dual-Lane Drive-Thru Impacts									
	Percentage of	Percentage of	Percentage of						
Poforo	customers who use	customers who use	customers who use						
Before	the Drive-Thru	Carry Out	Dine-In						
	55%	22%	23%						
	Percentage of	Percentage of	Percentage of						
A £4 a sr	customers using the	customers using	customers using						
After	Drive-Thru	Carry Out	Dine-In						
	75%	15%	10%						

Based on the data provided, Chick-fil-A feels that the site provides adequate parking as proposed and respectfully requests approval of the noted (15.5%) parking reduction.

Sincerely,

Jason Toole, P.E.

### CONSTRUCTION PLANS FOR





**MAPS** 

**VICINITY MAP** 

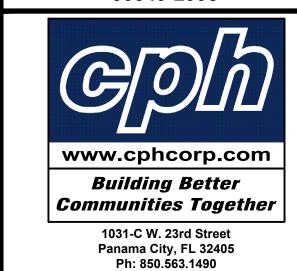
**LOCATION MAP** 

SCALE: 1" = 500'

SCALE: 1" = 3,000'



**5200 Buffington Road** Atlanta, Georgia 30349-2998



Plans Prepared By:

CPH, LLC A Full Service A & E Firm

JASON L. TOOLE, P.E.

is item has been digitally signed and sealed by Jason L. Too PF on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the

RELEASE: vX.YY.MM REVISION SCHEDULE **DESCRIPTION** C291126 Permit

3/29/2023 CPH, LLC Information contained on this drawing and in all digital files produced for above named project may not be reproduced in

erm **COVER SHEET** 

> SHEET NUMBER C-1.0

### 17365 CHESTERFIELD AIRPORT ROAD, CHESTERFIELD, ST. LOUIS COUNTY, MO, 63005 **STORE # 01976 SECTION 125 - TOWNSHIP 45 NORTH- RANGE 4 EAST**

**PARCEL ID: 17U330189** 

### **OWNER**

CHICK-FIL-A, INC. **5200 BUFFINGTON ROAD ATLANTA, GEORGIA 30349** ATTN.: MICAH DOWDY (678) 764-6637 micah.dowdy@cfacorp.com

CONSULTANTS

### **ENGINEER**

CPH, LLC. 1031-C WEST 23RD STREET PANAMA CITY, FLORIDA 32405 ATTN.: JASON L. TOOLE, P.E. (850) 563-1490 jtoole@cphcorp.com

### **SURVEYOR**

SHERRILL ASSOCIATES INC. 316 MAIN STREET, EDWARDSVILLE, IL. 62025 ATTN.: DAVID J. SHERRILL (618) 656-9251

### **ARCHITECT**

CPH, LLC. **500 WEST FULTON STREET** SANFORD, FLORIDA 32771 ATTN.: BROOK K. SHERRARD, AIA, NCARB, LEED AP (407) 322-6841 bsherrard@cphcorp.com

### **LANDSCAPE ARCHITECT**

**MANLEY LAND DESIGN 51 OLD CANTON ST. ALPHARETTA, GA 30009** ATTN.: AARON NEITZKE, RLA, (770) 442-8171 EXT 102

**ELECTRIC AMEREN CORPORATION** PO BOX 66149 ST.LOUIS, MO 63166 (314) 342-1111 myhomeamerenmissouri@ameren.com

### TELEPHONE/CABLE/FIBER

**UTILITY PROVIDERS** 

**SOUTHWESTERN BELL TELEPHONE** COMPANY 12851 MANCHESTER ROAD SUITE 1E **ST.LOUIS, MO 63131** (314) 505-0843

### **WATER & SEWER**

**MISSOURI-AMERICAN WATER COMPANY** 727 CRAIG ROAD **ST.LOUIS, MO 63141 ATTN.: DELLA STRODER** (866) 430-0820 della.stroder@amwater.com

### **STORM DRAINAGE**

CITY OF CHESTERFIELD PUBLIC WORKS 690 CHESTERFIELD PARKWAY W CHESTERFIELD, MO 63017 ATTN.: JIM ECKRICH (636) 537-4764 jeckrich@chesterfield.mo.us

### **APPROVAL AGENCIES**

### **CITY OF CHESTERFIELD**

**PLANNING AND ZONING** 

CITY OF CHESTERFIELD

690 CHESTERFIELD PARKWAY W **CHESTERFIELD, MO 63017** ATTN.: CHRISTINE DOROUGH (636) 537-4741 cdorough@chesterfield.mo.us **BUILDING DEPARTMENT** CITY OF CHESTERFIELD 690 CHESTERFIELD PARKWAY W **CHESTERFIELD, MO 63017 ATTN.: JUSTIN WYSE** (636) 537-4000 pdsdirector@chesterfield.mo.us

### **HEALTH DEPARTMENT**

ST. LOUIS COUNTY DEPARTMENT OF PUBLIC HEALTH 6121 N. HANLEY ROAD **ATTN.: MS. CATHERINE** (314) 615-0894 mredecker@stlouiscountymo.gov

### **COVER SHEET**

**INDEX OF SHEETS** 

SHEET-1 OF 1 ALTA/NSPS LAND TITLE SURVEY SITE PLAN PARKING EASEMENT EXHIBIT SITE LIGHTING PHOTOMETRIC PLAN SITE LIGHTING CUT SHEETS LANDSCAPE PLAN

### This Site Development Plan was approved by the City of Chesterfield Planning Commission and Commission, authorizing the recording of this Site Development Plan pursuant to Chesterfield Ordinance Number 200, as attested to by the Director of Planning and the City Clerk. Justin Wyse, AICP Director of Planning City of Chesterfield, Missou

(AND INCLUDE THIS SECTION)

Rev. 02/2020

### (AND EITHER INCLUDE THIS SECTION FOR A CORPORATION) On this 13th day of June \_, A.D., 2023, before me personally appeared Chesterfield Six Development, L.L.C. that he/she is the Manager I myed histing comparation in the State of Missouri , and that the seal affixed to the foregoing instrum is the corporate seal of said corporation, and that said instrument was signed on behalf of said corporation by authority of its Board of Directors; and the said Cobert J Jokheut in the first acknowledged said instrument to be the free act and deed of said corporation. In Testimony Whereof, I have hereunto set my hand and affixed my Notarial Seal at my Office in Shibois County, Missouri, the day and year last above written. My term expires 11 07 3036 (OR INCLUDE THIS SECTION FOR AN INDIVIDUAL)

Rev. 02/2020

### to be the person(s) described in, and who executed the foregoing instrument, and acknowledged that he, she or they executed the same as his, her, or their free act and deed In Testimony Whereof, I have hereunto set my hand and affixed my Notarial Seal at my Office in \_, the day and year last above written. My term expires

Rev. 02/2020

**Chesterfield** DEPARTMENT OF PLANNING SCRIPT FOR A SITE DEVELOPMENT PLAN

THF Chesterfield Six Development, L.L.C., the owner(s) of the property shown on this plan for and in

consideration of being granted approval of said plan to develop property under the provisions of Section 03.\_\_\_\_\_\_, District of City of Chesterfield Unified Development

Code, do hereby agree and declare that said property from the date of recording this plan shall be developed only as shown thereon, unless said plan is amended by the City of Chesterfield, or

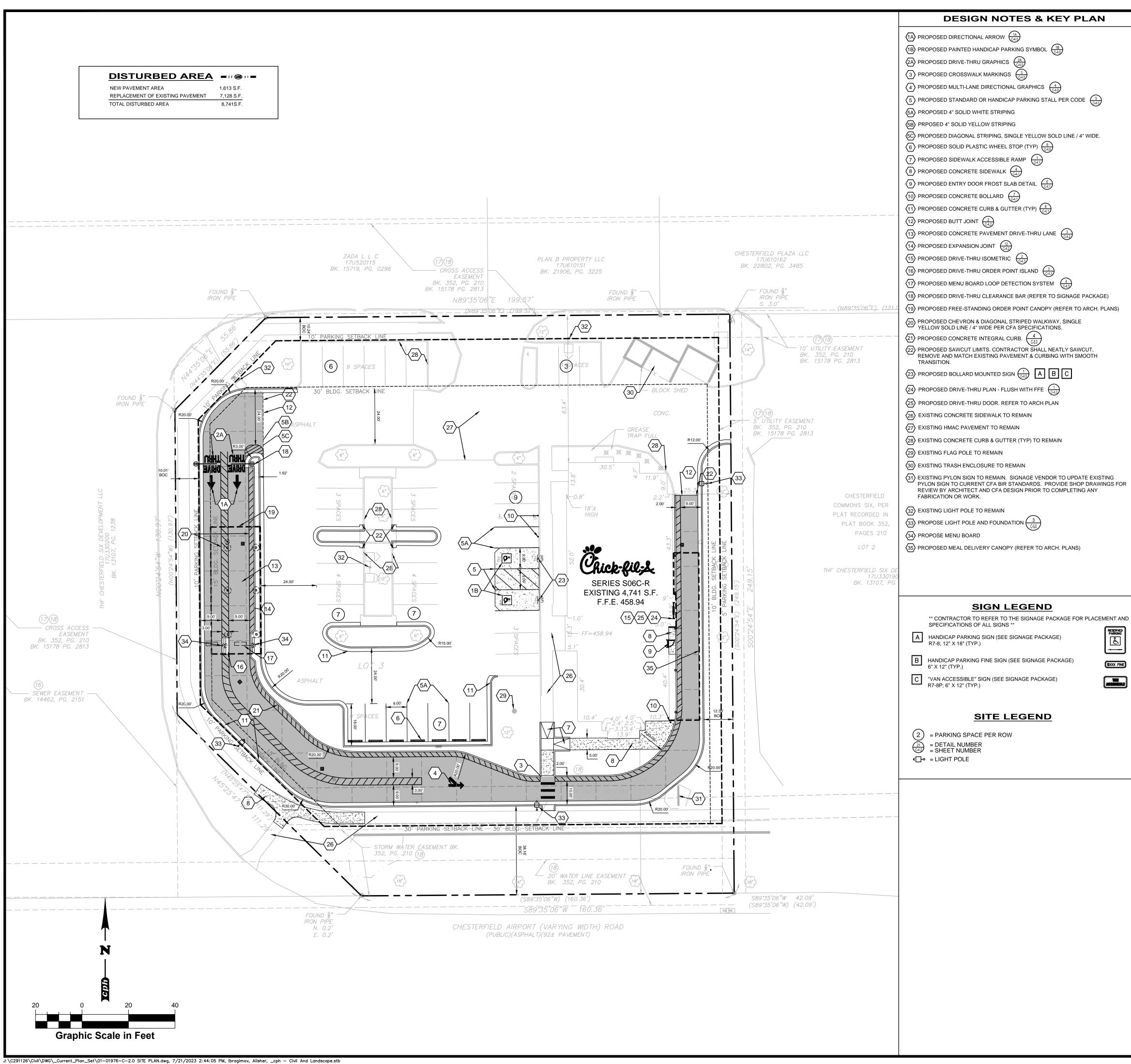
See attached Legal Description

voided or vacated by order of ordinance of the City of Chesterfield Counc

(Notary Public)

Robert J. Jakubeck, as Manager of

(Name Typed): Milan Green Management, L.L.C.



### SITE DATA

(1B) PROPOSED PAINTED HANDICAP PARKING SYMBOL (B) (C-4.0)

(2A) PROPOSED DRIVE-THRU GRAPHICS (2A)

(3) PROPOSED CROSSWALK MARKINGS  $(\frac{3}{6.40})$ 

4 PROPOSED MULTI-LANE DIRECTIONAL GRAPHICS 4

 $\langle 5 \rangle$  PROPOSED STANDARD OR HANDICAP PARKING STALL PER CODE  $\frac{5}{(240)}$ 

(5C) PROPOSED DIAGONAL STRIPING, SINGLE YELLOW SOLD LINE / 4" WIDE.

 $\langle 6 \rangle$  PROPOSED SOLID PLASTIC WHEEL STOP (TYP)  $\langle 6 \rangle$ 

(8) PROPOSED CONCRETE SIDEWALK (4)

9 PROPOSED ENTRY DOOR FROST SLAB DETAIL  $\frac{6}{0.41}$ 

10) PROPOSED CONCRETE BOLLARD (7)

(11) PROPOSED CONCRETE CURB & GUTTER (TYP)  $\frac{8}{(C-4.1)}$ 

(13) PROPOSED CONCRETE PAVEMENT DRIVE-THRU LANE (3)

(14) PROPOSED EXPANSION JOINT (12)

(15) PROPOSED DRIVE-THRU ISOMETRIC (2) (2-4.3)

(18) PROPOSED DRIVE-THRU CLEARANCE BAR (REFER TO SIGNAGE PACKAGE)

(19) PROPOSED FREE-STANDING ORDER POINT CANOPY (REFER TO ARCH. PLANS)

PROPOSED CHEVRON & DIAGONAL STRIPED WALKWAY, SINGLE YELLOW SOLD LINE / 4" WIDE PER CFA SPECIFICATIONS.

(22) PROPOSED SAWCUT LIMITS. CONTRACTOR SHALL NEATLY SAWCUT, REMOVE AND MATCH EXISTING PAVEMENT & CURBING WITH SMOOTH

23) PROPOSED BOLLARD MOUNTED SIGN (749) A B C

24 PROPOSED DRIVE-THRU PLAN - FLUSH WITH FFE  $\frac{1}{0.43}$ 

25) PROPOSED DRIVE-THRU DOOR. REFER TO ARCH PLAN

(26) EXISTING CONCRETE SIDEWALK TO REMAIN

(27) EXISTING HMAC PAVEMENT TO REMAIN

(28) EXISTING CONCRETE CURB & GUTTER (TYP) TO REMAIN

(31) EXISTING PYLON SIGN TO REMAIN. SIGNAGE VENDOR TO UPDATE EXISTING PYLON SIGN TO CURRENT CFA BIR STANDARDS. PROVIDE SHOP DRAWINGS FOR REVIEW BY ARCHITECT AND CFA DESIGN PRIOR TO COMPLETING ANY

 $\langle 33 \rangle$  PROPOSE LIGHT POLE AND FOUNDATION  $\begin{pmatrix} 3 \\ 0.50 \end{pmatrix}$ 

(35) PROPOSED MEAL DELIVERY CANOPY (REFER TO ARCH. PLANS)

### **SIGN LEGEND**

SPECIFICATIONS OF ALL SIGNS \*\*

A HANDICAP PARKING SIGN (SEE SIGNAGE PACKAGE)

### SITE LEGEND

### **STATEMENT OF INTENT**

PROPOSED PROJECT IS PART OF OWNER'S EXISTING PROPERTY REINVESTMENT PROGRAM TO IMPROVE CUSTOMER SERVICE AND RESTAURANT OPERATIONS. PROPERTY IMPROVEMENTS WILL INCLUDE DUAL LANE DRIVE THRU WITH CANOPY AND ENTRANCE DRIVE RELOCATION. EXISTING PARKING LOT WILL BE ADJUSTED TO SUPPORT ELEMENTS PROPOSED.

ADDRESS: 17365 CHESTERFIELD AIRPORT ROAD CHESTERFIELD, MO 63005

CHICK-FIL-A SITE AREA: 1.28 AC.± (55,757 S.F. ±)

**ZONING: C8: PLANNED COMMERCIAL** 

### PARCEL ID: 17U330189 **BUILDING INFORMATION**

EXISTING & PROPOSED CONDITION: EXISTING BUILDING AREA: 4,741 S.F. **EXISTING FLOOR AREA RATIO: .085** NUMBER OF STORIES: 1

### LAND COVERAGE SUMMARY

EXISTING CONDITION: IMPERVIOUS AREA: 39,171 S.F. 70.25(%) OPEN SPACE: TOTAL:

PROPOSED CONDITION: IMPERVIOUS AREA: 39,022 S.F. 69.99(%) OPEN SPACE: 16,735 S.F. 30.01(%) 55,757 S.F. (100.00%

BUILDING SETBACKS FRONT (SOUTH) SIDE (WEST) 167.44 SIDE (EAST) REAR (NORTH)

PARKING SETBACK FRONT (SOUTH) SIDE (WEST) SIDE (EAST) 11.39' REAR (NORTH)

### **PARKING INFORMATION**

15 SPACES /1000 SF = 4,741 SF/1000 SF X 15 = 71 SPACES 8 % REDUCTION ALLOWED = 71 SPACES - 8% = 65 SPACES REQUIRED AN ADDITIONAL REDUCTION .

### PARKING PROVIDED

REGULAR 37 SPACES OFF-SITE 21 SPACES

EXISTING PARKING

REGULAR 51 SPACES

HANDICAP TOTAL

CFA HAS ACCESS TO CROSS PARKING EASEMENT TO SATISFY PARKING REQUIREMENT. 21 PARKING SPACES AVAILABLE.

### **GENERAL NOTES**

DIMENSIONS SHOWN ARE TO THE FACE OF CURB UNLESS SHOWN (BOC) WHICH INDICATES BACK OF CURB.

2. ALL CURBED RADII ARE TO BE 10' AND 3' UNLESS OTHERWISE NOTED.

B. ALL SIGNS SHALL HAVE A 7'0" MIN. CLEARANCE FROM FINISH GRADE TO BOTTOM OF LOWEST SIGN MOUNTED ON POST. ALL SIGNAGE INCLUDING REGULATORY SIGNAGE,

SHALL MEET THE LOCAL AGENCY DESIGN GUIDELINES & MUTCD REQUIREMENTS.

4. ALL SIGNS SHALL BE PROVIDED AND INSTALLED BY THE CONTRACTOR.

5. ALL STRIPING SHALL BE IN CONFORMANCE WITH LOCAL AGENCY REQUIREMENTS.

. ADJUST ALL MANHOLE RINGS AND CLEAN OUT COVERS TO BE FLUSH WITH FINISHED GRADE. ALL PARTS TO BE TRAFFIC BEARING (AASHTO H-20 LOADING). MANHOLE COVERS SHALL MEET THE LOCAL AGENCY STANDARD.

TRANSITIONS TO BE FLUSH ALL ALONG THE ACCESSIBLE ROUTE. CONTRACTOR SHALL ENSURE THE MAXIMUM CONSTRUCTED LEVEL TRANSITION AT EVERY JOINT DOES NOT EXCEED THE MAXIMUM ALLOWABLE UNDER THE ADA AFTER SETTLEMENT, EXPANSION, CONTRACTION, ETC. CHANGES IN LEVEL OF 1/4" HIGH MAXIMUM SHALL BE PERMITTED TO BE VERTICAL. CHANGES IN LEVEL BETWEEN 1/4" HIGH MINIMUM AND 1/2" HIGH MAXIMUM SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 1:2.

B. ALL CONCRETE POURS SHALL BE BOUND BY EXPANSION JOINTS WHEN ABUTTING ANOTHER CONCRETE POUR / SLAB OR ASPHALT PAVEMENT, INCLUDING CURBING.

. ALL AFFECTED SIDEWALKS, RAMPS AND CROSSWALKS WILL BE BUILT AND INSPECTED TO MEET CURRENT ADA REQUIREMENTS.

10. ALL EXISTING STRIPING SHALL BE RE-STRIPED TO MATCH EXISTING CONDITIONS UNLESS NOTED OTHERWISE ON PLAN.





**5200 Buffington Road** Atlanta, Georgia 30349-2998



1031-C W. 23rd Street Panama City, FL 32405 Ph: 850.563.1490

Plans Prepared By: CPH, LLC A Full Service A & E Firm

> JASON L. TOOLE, P.E. 2023009223

nis item has been digitally signed and sealed by Jason L. Toole PE. on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

# 300

DESCRIPTION

RELEASE: **REVISION SCHEDULE** 

CONSULTANT PROJECT # C291126

Permit 3/29/2023 CPH, LLC

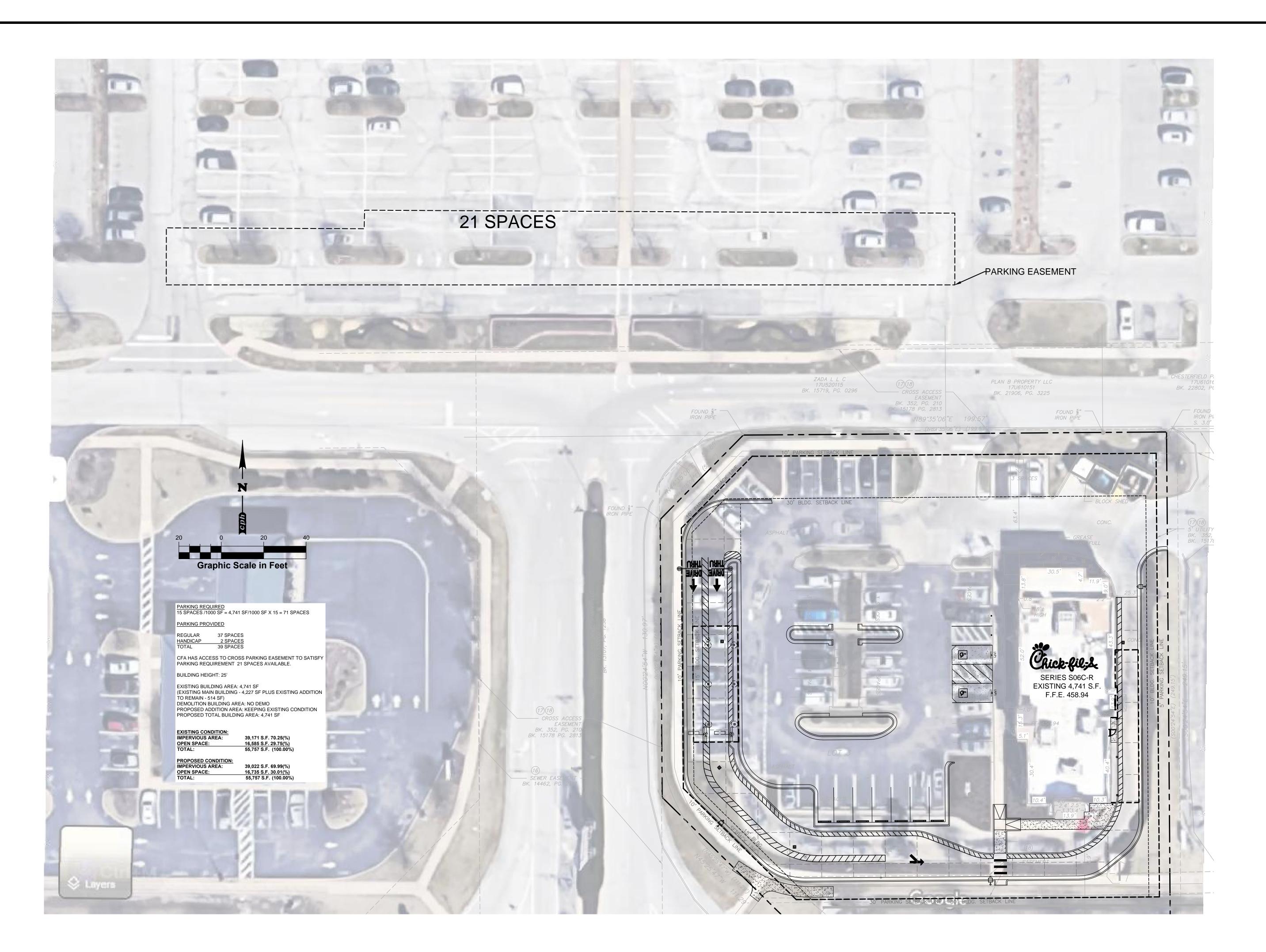
Information contained on this drawing and in all digital files

produced for above named project many manner without express written cauthorized project representatives.

SHEET

AMENDED SITE produced for above named project may not be reproduced in any manner without express written or verbal consent from **DEVELOPMENT SECTION** PLAN

SHEET NUMBER



J:\C291126\Civil\DWG\\_Current\_Plan\_Set\01-01976-C-2.0 SITE PLAN.dwg, 7/21/2023 9:43:13 AM, Ibragimov, Alisher, \_cph - Civil And Landscape.stb





30349-2998

1031-C W. 23rd Street Panama City, FL 32405 Ph: 850.563.1490

Plans Prepared By: CPH, LLC A Full Service A & E Firm

JASON L. TOOLE, P.E. 2023009223

This item has been digitally signed and sealed by Jason L. Toole PE. on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

# 63005

BUILDING TYPE / SIZE: RELEASE: vX.YY.MM **REVISION SCHEDULE DESCRIPTION** CONSULTANT PROJECT # C291126 Permit 3/29/2023

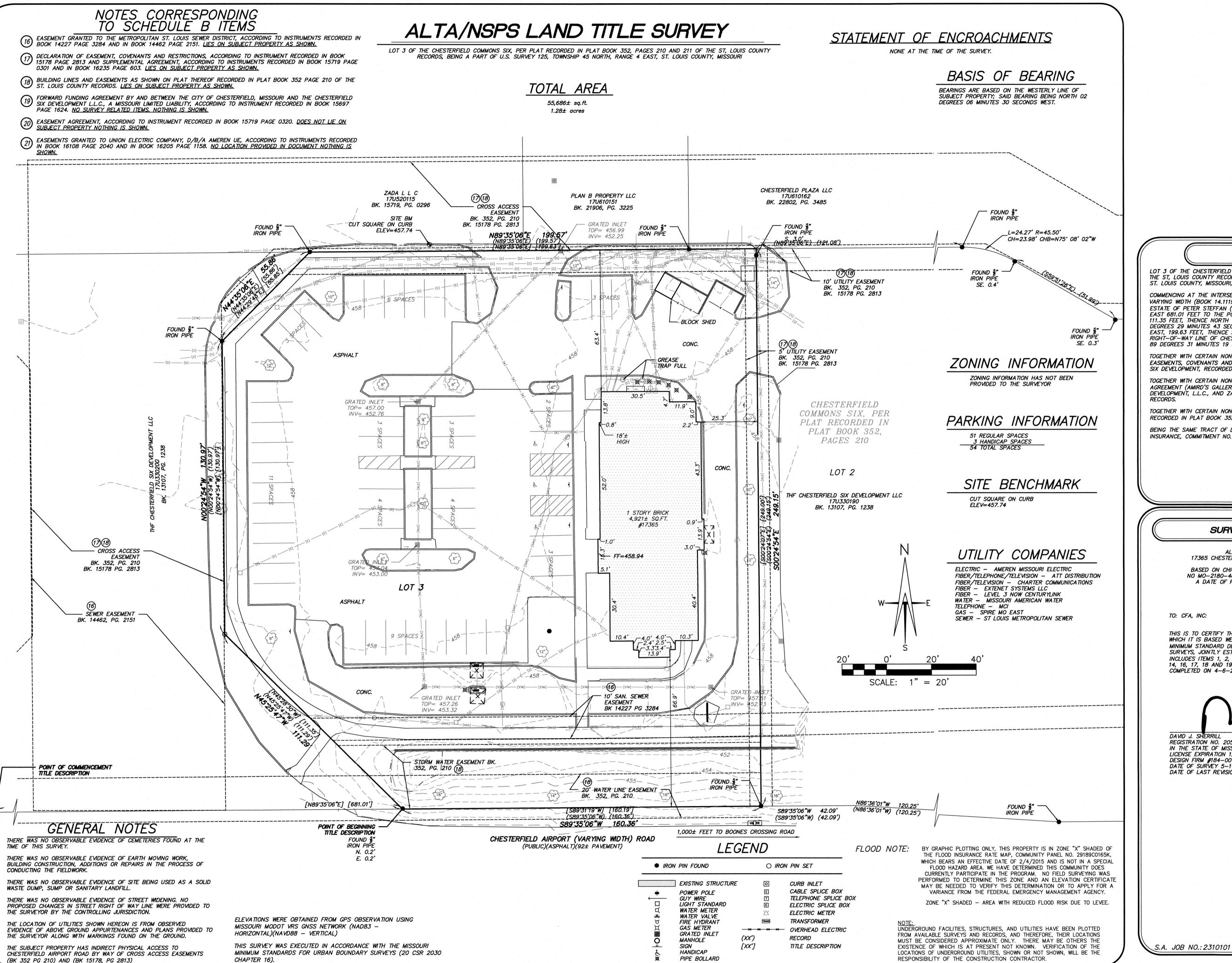
CPH, LLC Information contained on this drawing and in all digiproduced for above named project may not be reprany manner without express written or verbal consequences.

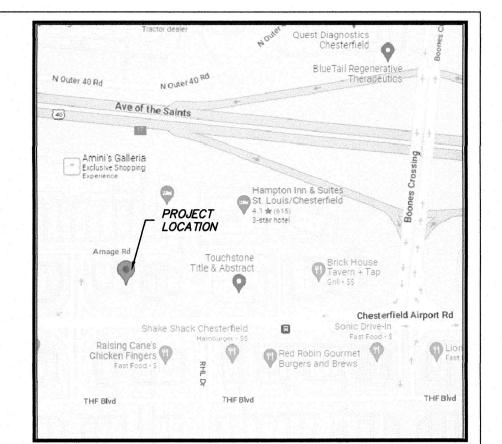
SHEET

PARKING EASEMENT

EXHIBIT Information contained on this drawing and in all digital files produced for above named project may not be reproduced in any manner without express written or verbal consent from authorized project representatives.

SHEET NUMBER





LOCATION MAP (NOT TO SCALE)

### LEGAL DESCRIPTION

LOT 3 OF THE CHESTERFIELD COMMONS SIX, PER PLAT RECORDED IN PLAT BOOK 352, PAGES 210 AND 211 OF THE ST, LOUIS COUNTY RECORDS, BEING A PART OF U.S. SURVEY 125, TOWNSHIP 45 NORTH, RANGE 4 EAST, ST. LOUIS COUNTY, MISSOURI, AND BEING MORE PARTICULARLY DESCRIBED AS:

COMMENCING AT THE INTERSECTION OF THE NORTH RIGHT-OF-WAY LINE OF CHESTERFIELD AIRPORT ROAD, VARYING WIDTH (BOOK 14.1119, PAGE 660), AND THE EAST LINE OF SHARE NO. 1 OF THE SUBDIVISION OF THE ESTATE OF PETER STEFFAN (BOOK 80, PAGE 457), THENCE NORTH 89 DEGREES 35 MINUTES 06 SECONDS EAST 681.01 FEET TO THE POINT OF BEGINNING, THENCE NORTH 45 DEGREES 28 MINUTES 30 SECONDS WEST 111.35 FEET, THENCE NORTH OO DEGREES 24 MINUTES 54 SECONDS WEST 130.97 FEET, THENCE NORTH 44 DEGREES 29 MINUTES 43 SECONDS EAST 55.85 FEET, THENCE NORTH 89 DEGREES 35 MINUTES 06 SECONDS EAST, 199.63 FEET, THENCE SOUTH OO DEGREES 24 MINUTES O7 SECONDS EAST 249.00 FEET TO THE NORTH RIGHT-OF-WAY LINE OF CHESTERFIELD AIRPORT ROAD, THENCE ALONG SAID NORTH RIGHT-OF-WAY LINE SOUTH 89 DEGREES 31 MINUTES 19 SECONDS WEST 160.19 FEET TO THE POINT: OF BEGINNING.

TOGETHER WITH CERTAIN NON-EXCLUSIVE EASEMENTS CONTAINED IN THAT CERTAIN DECLARATION OF EASEMENTS, COVENANTS AND RESTRICTIONS (CHESTERFIELD SIX) DATED JUNE 19, 2003 BY THE CHESTERFIELD SIX DEVELOPMENT, RECORDED IN BOOK 15178 PAGE 2813, OF THE ST. LOUIS COUNTY RECORDS.

TOGETHER WITH CERTAIN NON-EXCLUSIVE PARKING EASEMENTS CONTAINED IN THAT CERTAIN SUPPLEMENTAL AGREEMENT (AMIRD'S GALLERIA) DATED MARCH 23, 2004 BY AND BETWEEN THE CHESTERFIELD SIX DEVELOPMENT, L.L.C., AND ZADA. L.L.C., RECORDED IN BOOK 15719 PAGE 301 OF THE ST. LOUIS COUNTY

TOGETHER WITH CERTAIN NON-EXCLUSIVE INGRESS AND EGRESS EASEMENTS GRANTED IN THAT CERTAIN PLAT RECORDED IN PLAT BOOK 352 PAGE(S) 210 AND 211, OF THE ST. LOUIS COUNTY RECORDS.

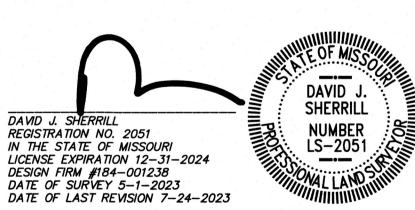
BEING THE SAME TRACT OF LAND DESCRIBED IN A TITLE REPORT PREPARED BY FIRST AMERICAN TITLE INSURANCE, COMMITMENT NO. NCS--1165334-CO DATED 1/20/2022.

### SURVEYOR'S CERTIFICATE

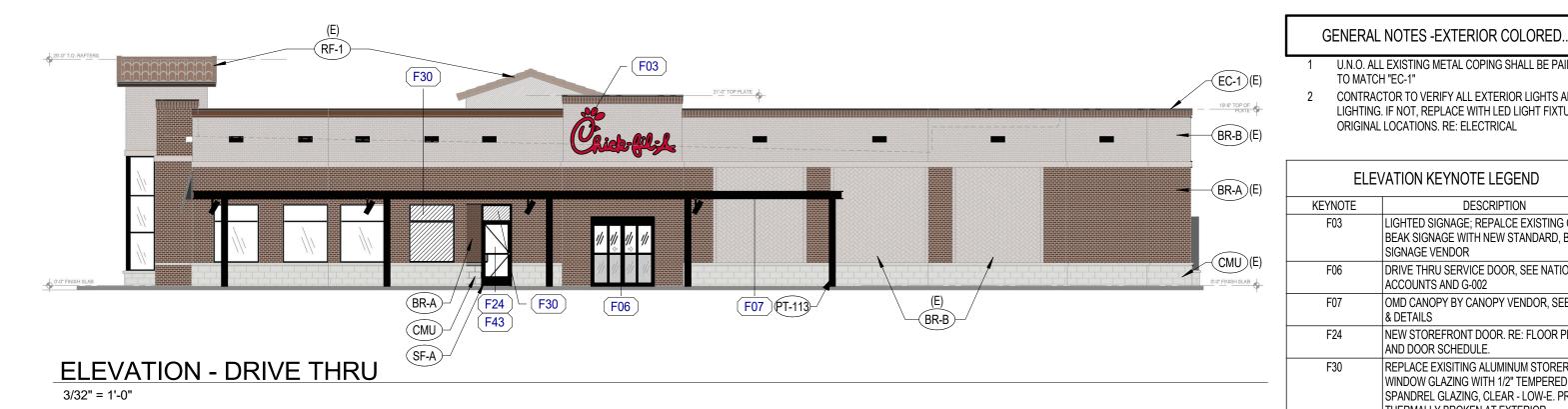
ALTA/NSPS LAND TITLE SURVEY 17365 CHESTERFIELD AIRPORT ROAD, CHESTERFIELD, MO BASED ON CHICAGO TITLE INSURANCE COMPANY, POLICY NO MO-2180-46-6-04105-2007.7210672-73369630 WITH A DATE OF POLICY OF MARCH 19, 2007 AT 9:15 AM

TO: CFA, INC:

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2021 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 5, 6(a), 7(b)(1), 7(c), 8, 9, 10, 11a, 11b, 13, 14, 16, 17, 18 AND 19 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON 4-6-2023.



V



### (E) RF-1 F03 (E) (EC-1) (E) (BR-B) (E) (BR-A) (E) (CMU (E) F44 BR-B

### **ELEVATION - ENTRY**

3/32" = 1'-0"

### **EXTERIOR COLORED ELEVATIONS**



U.N.O. ALL EXISTING METAL COPING SHALL BE PAINTED

CONTRACTOR TO VERIFY ALL EXTERIOR LIGHTS ARE LED

LIGHTING. IF NOT, REPLACE WITH LED LIGHT FIXTURES IN

DESCRIPTION

LIGHTED SIGNAGE; REPALCE EXISTING OPEN

BEAK SIGNAGE WITH NEW STANDARD, BY

DRIVE THRU SERVICE DOOR, SEE NATIONAL

NEW STOREFRONT DOOR. RE: FLOOR PLAN

REPLACE EXISITING ALUMINUM STORERONT WINDOW GLAZING WITH 1/2" TEMPERED

SPANDREL GLAZING, CLEAR - LOW-E. PROVIDE

REMOVE EXISTING STORE HOURS VINYL SIGN

AND APPLY 3M 7725-10 OPAQUE WHITE VINYL

SIGNAGE WITH NEW STORE HOURS TO EXTERIOR SURFACE OF EXTERIOR ENTRY DOOR AS REQUIRED. VERIFY NEW STORE HOURS DIFFER FROM EXISTING BEFORE

REMOVAL/REPLACEMENT.

THERMALLY BROKEN AT EXTERIOR LOCATIONS. SOLARBAN 90 W/ ICD COATING #3-0770 WARM GRAY ON INTERIOR SIDE.

APPLY 3M 7725-10 OPAQUE WHITE VINYL SIGNAGE SAYING "TEAM MEMBERS ONLY" TO EXTERIOR SURFACE OF EXTERIOR DRIVE THRU

OMD CANOPY BY CANOPY VENDOR, SEE PLANS

ORIGINAL LOCATIONS. RE: ELECTRICAL

SIGNAGE VENDOR

& DETAILS

F43

F44

ACCOUNTS AND G-002

AND DOOR SCHEDULE.

**ELEVATION KEYNOTE LEGEND** 

TO MATCH "EC-1"



### **ELEVATION - FRONT**

3/32" = 1'-0"



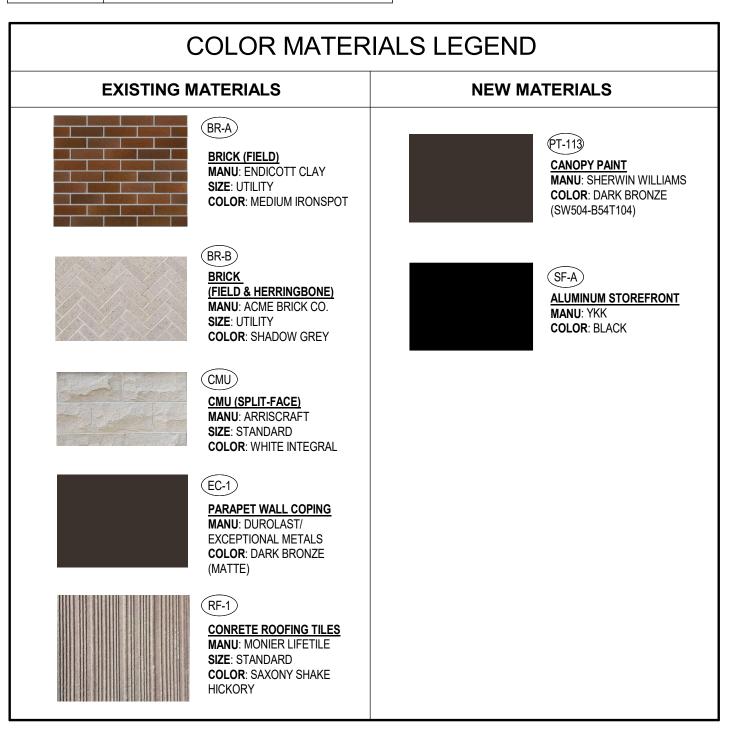
### **ELEVATION - REAR**

3/32" = 1'-0"

### **ELEVATION KEYNOTE LEGEND** KEYNOTE DESCRIPTION LIGHTED SIGNAGE: REPALCE EXISTING OPEN BEAK SIGNAGE F03 WITH NEW STANDARD, BY SIGNAGE VENDOR F07 OMD CANOPY BY CANOPY VENDOR, SEE PLANS & DETAILS REMOVE EXISTING STORE HOURS VINYL SIGN AND APPLY 3M F44 7725-10 OPAQUE WHITE VINYL SIGNAGE WITH NEW STORE HOURS TO EXTERIOR SURFACE OF EXTERIOR ENTRY DOOR AS REQUIRED. VERIFY NEW STORE HOURS DIFFER FROM EXISTING BEFORE REMOVAL/REPLACEMENT

### GENERAL NOTES -EXTERIOR COLORED..

- U.N.O. ALL EXISTING METAL COPING SHALL BE PAINTED TO MATCH "EC-1"
- CONTRACTOR TO VERIFY ALL EXTERIOR LIGHTS ARE LED LIGHTING. IF NOT, REPLACE WITH LED LIGHT FIXTURES IN ORIGINAL LOCATIONS. RE: ELECTRICAL



### **EXTERIOR COLORED ELEVATIONS & COLOR MATERIALS LEGEND**

### MIDWEST LANDSCAPE NOTES

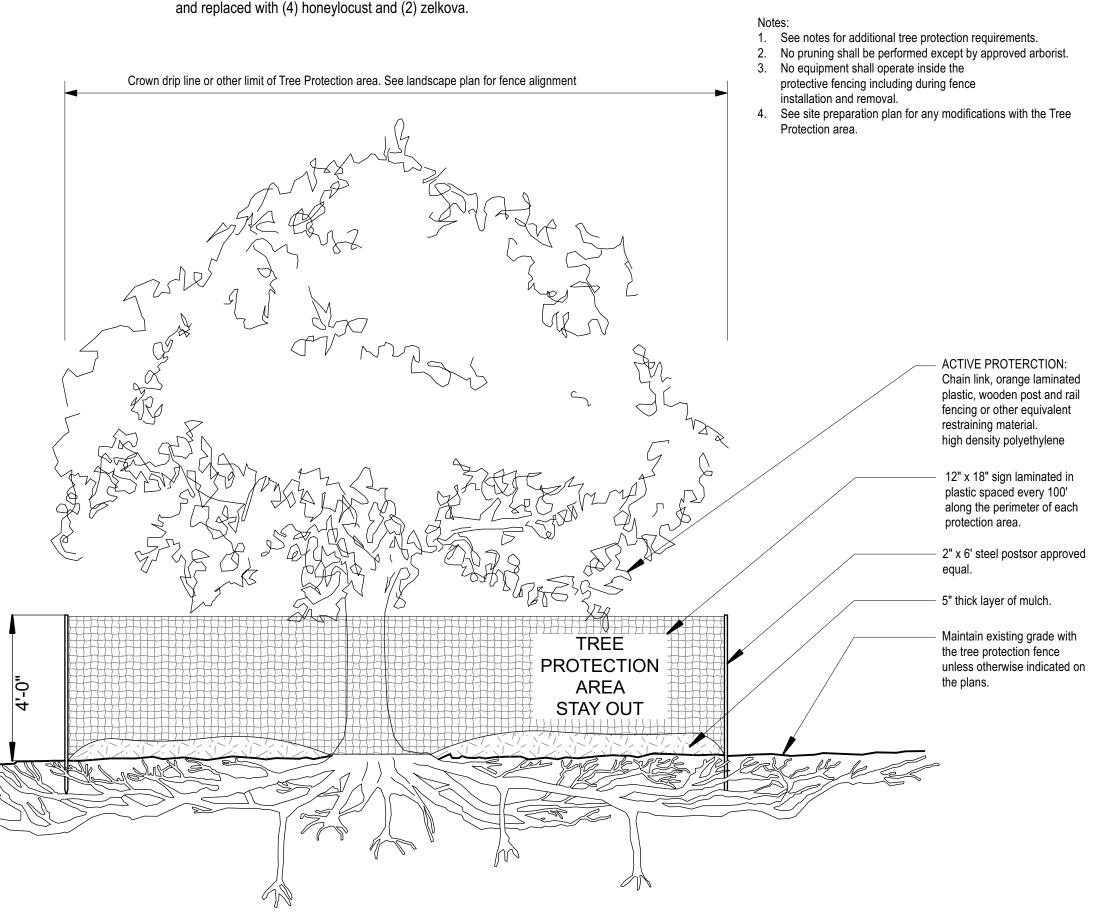
- 1. Landscape Contractor to read and understand the Landscape Specifications prior to finalizing bids. The Landscape Specifications shall be adhered to throughout the construction process.
- 2. Contractor is responsible for locating and protecting all underground
- utilities prior to digging. 3. Contractor is responsible for protecting existing trees from damage during construction.
- 4. All tree protection devices to be installed prior to the start of land disturbance, and maintained until final landscaping.
- 5. All tree protection areas to be protected from sedimentation. 6. All tree protection fencing to be inspected daily, and repaired or replaced
- 7. No parking, storage or other construction activities are to occur within tree
- protection areas. 8. All planting areas shall be cleaned of construction debris (ie. concrete, rock, rubble, building materials, etc) prior to adding and spreading of the
- 9. General Contractor is responsible for adding a min of 4" clean friable topsoil in all planting beds and all grassed areas. Graded areas to be held down the appropriate elevation to account for topsoil depth. See Landscape Specifications for required topsoil characteristics.
- 10. In all parking lot islands, the General Contractor is responsible to remove all debris, fracture/loosen subgrade to a min. 24" depth. Add topsoil to a 6"-8" berm height above island curbing; refer to landscape specifications and landscape island detail.
- 11. Prior to beginning work, the Landscape Contractor shall inspect the subgrade, general site conditions, verify elevations, utility locations, irrigation, approve topsoil provided by the General Contractor and observe the site conditions under which the work is to be done. Notify the General Contractor of any unsatisfactory conditions, work shall not proceed until such conditions have been corrected and are acceptable to
- the Landscape Contractor. 12. Any deviations from the approved set of plans are to be approved by the Landscape Architect.
- 13. Landscaping shall be installed in conformance with ANSI Z60.1 the "American Standard for Nursery Stock" and the accepted standards of the American Association of Nurserymen.

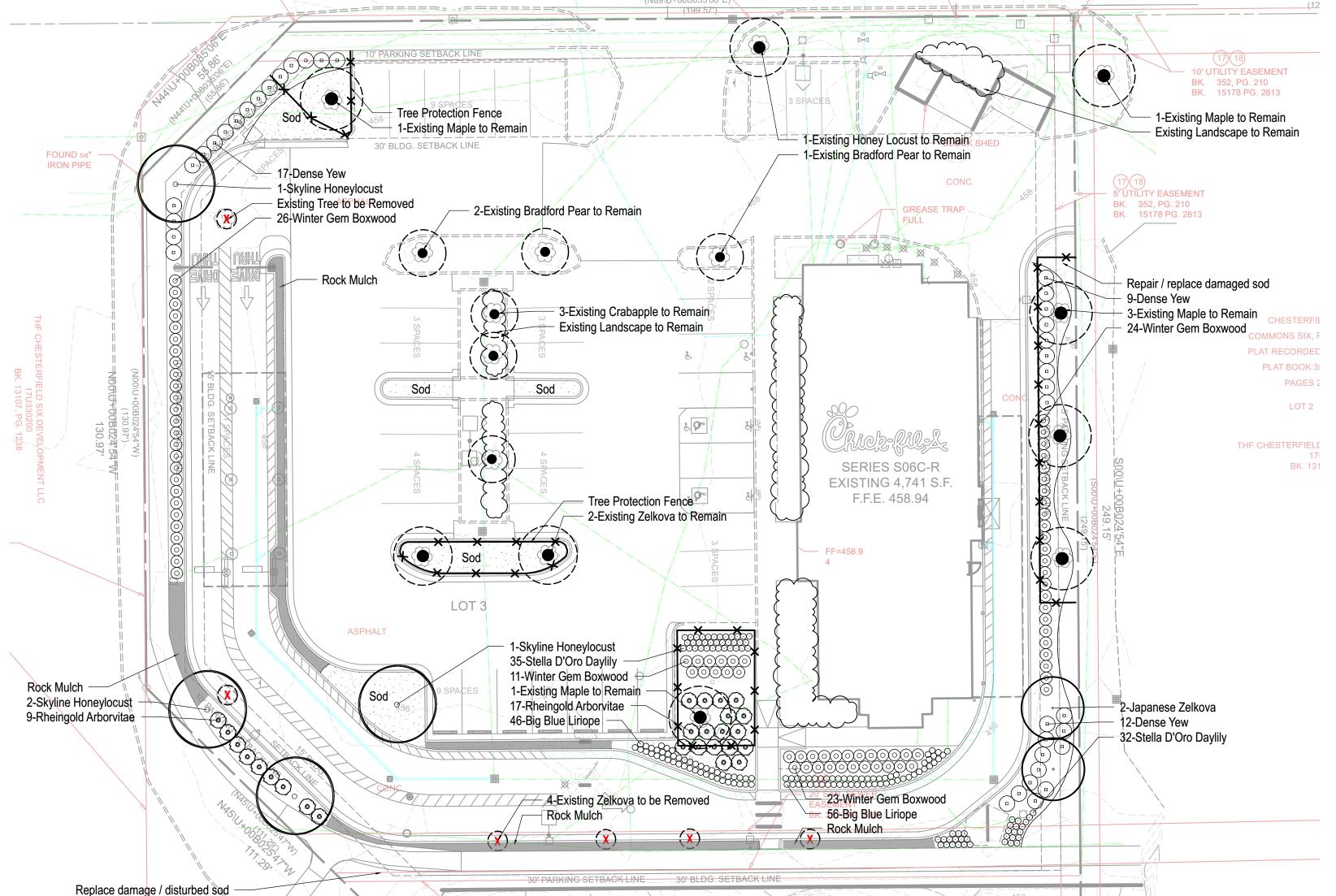
- 14. Existing grass in proposed planting areas shall be killed and removed. Hand rake to remove all rocks and debris larger than 1 inch in diameter, prior to adding topsoil and planting shrubs.
- 15. Soil to be tested to determine fertilizer and lime requirements prior to
- 16. Annual and perennial beds: add min. 4 inch layer of organic material and till to a min. depth of 12 inches. Mulch annual and perennial beds with 2-3 inch depth of mini nuggets.
- 17. All shrubs beds (existing and new) to be mulched with a min. 3 inch layer of mulch. Rock Mulch to be provided around the building as shown and called out on the landscape plan, all other planting beds are to be mulched with double shredded hardwood mulch.
- 18. Planting holes to be dug a minimum of twice the width of the root ball, for both shrub and tree. Set plant material 2-3" above finish grade. Backfill
- planting pit with topsoil and native excavated soil. 19. Sod to be delivered fresh (Cut less than 24 hours prior to arriving on site), laid immediately, rolled, and watered thoroughly immediately after planting. Edge of sod at planting beds are to be "V" trenched; see Landscape Details.
- 20. Any existing grass disturbed during construction to be fully removed, regraded and replaced. All tire marks and indentions to be repaired.
- 21. Water thoroughly twice in first 24 hours and apply mulch immediately. 22. The Landscape Contractor shall guarantee all plants installed for one full year from date of acceptance by the owner. All plants shall be alive and at a vigorous rate of growth at the end of the guarantee period. The Landscape Contractor shall not be responsible for acts of God or vandalism. See Landscape Specifications for Warranty
- requirements/expectations. 23. Any plant that is determined dead, in an unhealthy, unsightly condition, lost its shape due to dead branches, or other symptoms of poor, non-vigorous growth, shall be replaced by the Landscape Contractor. See Landscape Specifications for warranty requirements/expectations.
- 24. Site to be 100% irrigated in all planting beds and grass area by an automatic underground Irrigation System. Irrigation as-built shall be provided to the Landscape Architect within 24 hours of irrigation install
- 25. Stake all evergreen and deciduous trees as shown in the planting detail
- and as per the Landscape Specifications. 26. Remove stakes and guying from all trees after one year from planting.

### PLANT LIST

Qty	Botanical Name	Common Name	Scheduled Size	Remarks
	Trees			
4	Gleditsia triacanthos inermis 'Skyline'	Skyline Honeylocust	2.5" Cal; 12' Hgt.	B & B
2	Zelkova serrata 'Green Vase'	Green Vase Zelkova	2.5" Cal; 12' Hgt.	B & B
	Shrubs			
84	Buxus microphylla var. japonica 'Winter Gem'	Winter Gem Boxwood	3 Gal.	
38	Taxus x media 'Densiformis'	Dense Yew	3 Gal.	
26	Thuja occidentalis 'Rheingold'	Rheingold Arborvitae	3 Gal.	
	Groundcovers			
67	Hemerocallis 'Stella D'Oro'	Stella D'Oro Daylily	1 Gal.	Plant 18" O.C.
102	Liriope muscari 'Big Blue'	Big Blue Liriope	1 Gal.	Plant 18" O.C.
811	Sod to match existing	Sod to match existing	SF; Sod	
	Other			
762	Rock Mulch	Rock Mulch	SF.	See Specifications

(4) zelkova and (2) honeylocust existing to be removed





4-Existing Honeylocust to Remain

- CROSS ACCESS

BK. 352, PG. 210

EASEMENT

BK. 15178 RG8 3813+00B035'06"E

199.57

PLAN B PROPERTY LLC

17U610151

BK. 21906, PG. 3225

8K. 352, PG. 210

\$89\U+00B035'06"W

CHESTERFIELD AIRPORT (VARYING WIDTH) ROAD

(PUBLIC)(ASPHALT)(92± PAVEMENT)

FOUND 5/8" —

IRON PIPE

ZADALLC

BK. 15719, PG. 0296

FOUND 5/8" -

IRON PIPE

17U520115

352, PG. 210

FOUND 5/8"

**IRON PIPE** 

N. 0.2'

E. 0.2'





Atlanta, Georgia 30349-2998

CHESTERFIELD PLAZA LLC

BK. 22802, PG. 3485

— FOUND 5/8"

S. 3.0'

IRON PIPE

(\$89°35'06"W) (42.09')

**Call** before you dig.

(N89\U+00B(

Manley Land Design, Inc. 51 Old Canton Street Alpharetta, Georgia 30009 770.442.8171 tel



FSU# 01976

 NO.
 DATE 0/15/23
 BY Now Site Plan

 1
 6/15/23
 1
 New Site Plan

MLD PROJECT # 2023094 PERMIT PRINTED FOR 4/20/23 KCN DRAWN BY

Information contained on this drawing and in all digital files produced for above named project may not be reproduced in any manner without express written or verbal consent from authorized project representatives.

Landscape Plan

SHEET NUMBER L-100

**IRRIGATION SYSTEM RETROFIT** 1. Existing irrigation system shall be retrofitted to include 100% cover to all newly created landscaped areas.

2. Sprays and rotors shall be utilized on all sod and seedded areas.

- 3. Drip irrigation shall be utilized on all planting beds, shrubs, trees, and groundcover.
- 4. The Contractor shall be responsible to ensure new and existing irrigation components, and the system as a whole, are in
- 5. Add valves, sprays, rotors, drip, and/or replace zones as needed to achieve proper coverage as required.
- 6. Only RainBird products shall be utilized 7. Mainline shall be 1.5" CLS 200 PVC
- 8. Lateral lines shall be 1" CLS 200 PVC
- 9. Rain / freeze sensor shall be installed
- 10. Prior to final completion, the Irrigation Contractor shall perform a walk-thru inspection with the Owner, or Owners

### **NEW FIXTURE** INSTALLED ON EXISTING POLE AND BASE. 0.5 1.5 1.4 1.3 1.2 1.0 0.7 PRV-C40-T4 NEW FIXTURE INSTALLED ON EXISTING 2.0 1.7 1.4 1.3 1.2 1.2 1.3 POLE AND BASE. 2.0 1.8 1.5 1.4 1.4 1.4 1.4 1.4 1.9 2.3 2.6 2.3 2.2 2.3 2.3 PRV-C40-T4 MH: 30 1.9 2.1 2.2 2.3 2.3 2.4 2.4 2.5 2.2 2.3 2.6 2.7 2.5 2.4 2.3 2.2 2.1 2.0 1.9 1.9 1.9 1.9 2.0 2.1 2.3 2.5 2.5 2.4 2.3 2.3 2.4 2.7 2.9 2.0 2.5 2.4 2.5 2.6 2.7 2.7 2.6 2.5 2.4 2.3 2.1 2.0 2.0 2.0 1.9 2.0 2.0 2.1 2.3 2.4 2.3 2.4 2.5 2.3 2.3 2.3 2.3 2.9 NEW FIXTURE INSTALLED <sup>2</sup>.0 <sup>2</sup>.0 <sup>1</sup>.9 <sup>1</sup>.9 <sup>2</sup>.0 ON NEW POLE AND BASE. 2.3 PRV-C40-T4 2.5 2.5 2.7 2.7 2.8 2.8 2.8 2.7 2.5 2.3 2.1 1.9 1.8 1.8 1.8 1.8 1.8 2.6 3.1 2.9 2.8 2.9 2.8 2.8 2.8 1.7 2.7 2.6 2.4 2.2 2.0 1.8 1.7 1.6 1.6 3.4 3.0 2.8 2.6 1.5 2.5 2.5 2.5 2.3 2.0 1.8 1.6 1.5 1.48.7 16.3 7.8 CRUS 4.2 3.4 3.2 2.7 2.7 1.5 2.6 2.5 2.7 2.5 2.1 1.8 1.6 1.5 1.4 NEW FIXTURES INSTALLED ON 13.8 <sup>3</sup>1.1 10.8 18.3 9.9 4.7 3.6 3.3 2.8 2.6 1.5 2.4 2.5 2.6 2.6 2.1 1.8 1.6 1.4 1.2 CRUS EXISTING 4,741 S.F. 14.5 4.7 3.5 3.2 2.8 2.7 1.5 2.6 2.6 2.6 2.4 2.0 1.7 1.5 1.5 1.3 1.5F.F.E. 458.94 4.5 3.4 3.2 3.0 2.7 1.6 2.5 2.5 2.4 2.2 1.9 1.6 1.5 1.3 1.1 1.0 CRUS <sup>†</sup>4.2 <sup>†</sup>3.2 <sup>†</sup>3.1 <sup>†</sup>3.0 2.3 2.0 1.7 1.6 1.4 $\frac{1}{2}$ 1.3 1.1 1.0 3.6 3.1 3.0 3.0 3.0 2.9 2.7 2.4 2.1 1.9 1.7 1.5 1.4 1.3 1.1 <sup>†</sup>10.9 3.5 3.7 3.5 3.2 3.0 3.0 2.9 2.9 2.7 2.5 2.3 2.1 1.9 1.7 1/6/ CRUS 14.7 25.9 25 2.4 2.4 2.4 2.7 2.4 2.4 2.3 2.2 2.1 2.0 2.0 1.9 2.4 2.4 2.3 2.3 2.5 PRV-C40-T4 2.2 2.2 2.1 2.1 2.0 2.1 2.2 NEW FIXTURE INSTALLED ON NEW POLE AND BASE. \_PRV-C40-T3 NEW FIXTURE INSTALLED

CHESTERFIELD AIRPORT (VARYING WIDTH) ROAD (PUBLIC)(ASPHALT)(92± PAVEMENT)

ON NEW POLE AND BASE.

### SITE LIGHTING PHOTOMETRIC PLAN SCALE: 1"=20'-0"

LUMINAIRE SCH	HEDULE					
SYMBOL	QTY	LABEL	ARRANGEMENT	LUMINAIRE	LLF	DESCRIPTION
				WATTS		
<b>○</b> —□ <b>&gt;</b>	1	PRV-C40-T3	SINGLE	131	1.000	PRV-C40-D-UNV-T3-BZ-HSS
<b>○</b>	4	PRV-C40-T4	SINGLE	131	1.000	PRV-C40-D-UNV-T4-BZ-HSS
	6	CRUS	SINGLE	74	0.850	CRUS-SC-LED-LW-30
<b>∢</b>	1	PRV-C60-T5	BACK-BACK	153	1.000	PRV-C60-D-UNV-T5 (2@180)

LABEL	CALCTYPE	UNITS	AVG	MAX	MIN	AVG/MIN	MAX/MIN
OMD CANOPY	ILLUMINANCE	FC	14.61	27.8	4.9	2.98	5.67
ORDER POINT CANOPY	ILLUMINANCE	FC	15.11	33.7	7.8	1.94	4.32
PARKING LOT	ILLUMINANCE	FC	2.25	4.7	1.0	2.25	4.70
PROPERTY LINE	ILLUMINANCE	FC	1.04	2.1	0.0	N/A	N/A

### GENERAL NOTES

- POLE DETAIL IS SHOWN FOR PRESENTATION PURPOSES ONLY AND CONTRACTOR SHALL VERIFY THAT EPA RATING OF LIGHTING FIXTURE DOES NOT EXCEED CAPABILITY OF POLE.
- PHOTOMETRIC PLAN IS PROVIDED FOR LIGHT PHOTOMETRIC DATA ONLY.
   CIRCUITING OF THE SITE LIGHTING SHALL BE SPECIFIED IN BUILDING
   ELECTRICAL DESIGN DRAWINGS.
- 3. PARKING AREA LIGHT FIXTURES ARE CLASSIFIED AS FULL CUTOFF FIXTURES PER IESNA LIGHT DISTRIBUTION GUIDELINES. ALL LIGHTING FIXTURES ARE DESIGNED, SHIELDED, AIMED AND LOCATED TO PREVENT LIGHT TRESPASS AND GLARE ONTO ADJACENT PROPERTIES OR ROADWAYS.
- 4. REFER TO BUILDING ELECTRICAL DRAWINGS FOR EXTERIOR LIGHTING CONTROL.





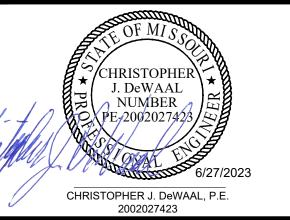
Chick-fil-A 5200 Buffington Road Atlanta, Georgia 30349-2998



Communities
Together

1031-C W. 23rd Street
Panama City, FL 32405
Ph: 850.563.1490

Plans Prepared By: CPH, LLC A Full Service A & E Firm



This item has been digitally signed and sealed by Christopher J. DeWaal, PE. on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

## **FIL-A ELD LD AO 63005**

UNICATERFIELD

17365 CHESTERFIELD

AIRPORT ROAD

CHESTERFIELD, MO

### SR#01976

BUILDING TYPE / SIZE: S06C-R
RELEASE: VX.YY.MM

REVISION SCHEDULE

NO. DATE DESCRIPTION

1
2
3
4

CONSULTANT PROJECT # C291126

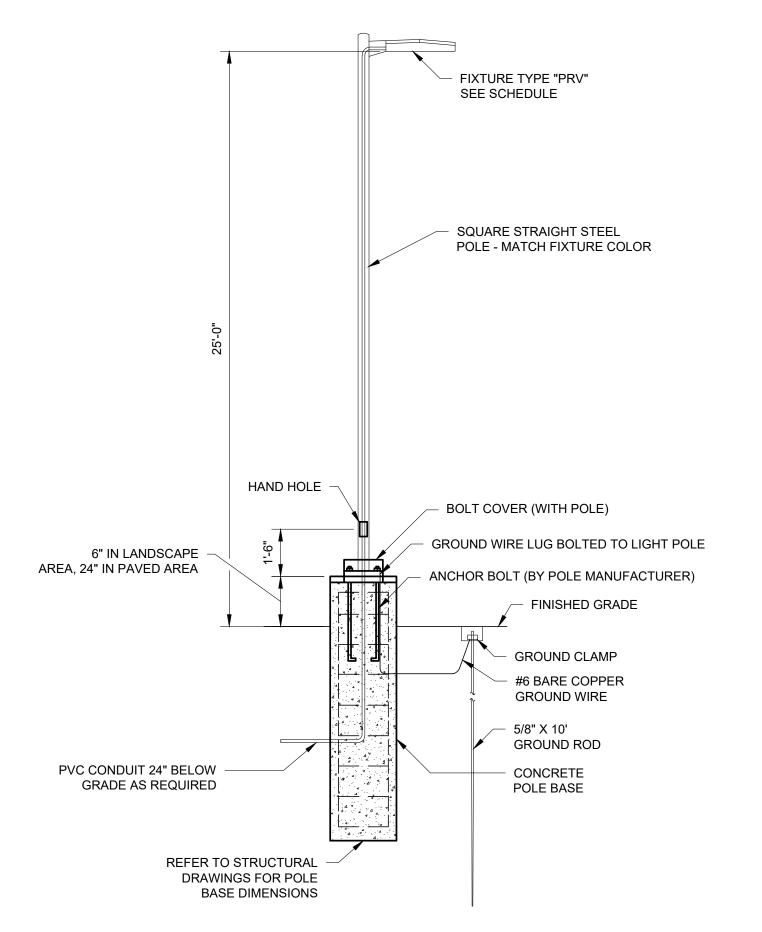
PRINTED FOR Permit

3/29/2023

Information contained on this drawing and in all digital files produced for above named project may not be reproduced in any manner without express written or verbal consent from authorized project representatives.

SHEET
SITE LIGHTING
PHOTOMETRIC PLAN

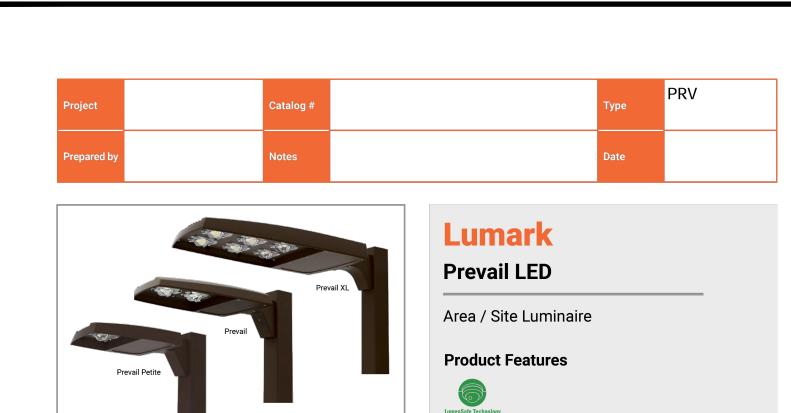
ES-1.0



**Graphic Scale in Feet** 

LIGHT FIXTURE/POLE DETAIL





### Interactive Menu

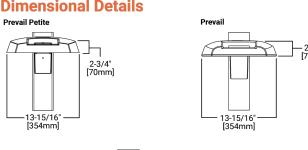
- Ordering Information page 2
- Mounting Details page 3 • Optical Configurations page 3
- Product Specifications page 3 Energy and Performance Data page 4
- Control Options page 5

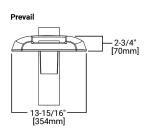
### **Quick Facts**

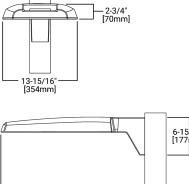
• Lumen packages range from 4,800 - 52,300 lumens (35W - 350W)

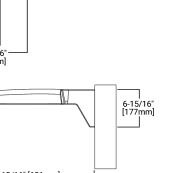
- Replaces 70W up to 1,000W HID equivalents Efficacies up to 160 lumens per watt
- Energy and maintenance savings up to 85% versus
- HID solutions
- Standard universal quick mount arm with universal

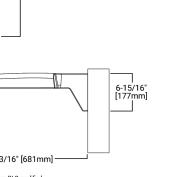
### drill pattern **Dimensional Details**

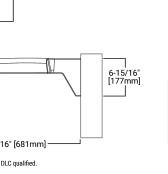


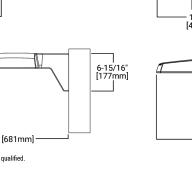






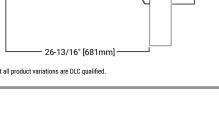






Connected Systems

WaveLinx





**Ordering Information** 



SAMPLE NUMBER: PRV-XL-C75-D-UNV-T4-SA-BZ										
Product Family <sup>1, 2</sup>	Light Engine <sup>3</sup>	Driver	Voltage	Distribution	Mounting	Color				
PRV-P=Prevail Petite BAA-PRV-P=Prevail Petite BAA Compliant <sup>23</sup> TAA-PRV-P=Prevail Petite TAA Compliant <sup>23</sup>	C10=(1 LED) 4,900 Nominal Lumens C15=(1 LED) 6,900 Nominal Lumens C20=(1 LED) 9,800 Nominal Lumens C25=(1 LED) 11,800 Nominal Lumens	D=Dimming (0-10V)	UNV=Universal (120-277V) 347=34/V 480=480V <sup>4</sup> DV=DuraVolt (277-480V) <sup>4,25</sup>	T2=Type II T3=Type III T4=Type IV T5=Type V	SA=Standard Versatile Arm MA=Mast Arm WM=Wall Mount Arm	BZ=Bronze AP=Grey BK=Black DP=Dark Platinum GM=Graphite Metallic				
PRV=Prevail BAA-PRV=Prevail BAA Compliant <sup>23</sup> TAA-PRV=Prevail TAA Compliant <sup>23</sup>	C15=(1 LED) 7,100 Nominal Lumens C25=(2 LEDs) 13,100 Nominal Lumens C40=(2 LEDs) 17,100 Nominal Lumens C60=(2 LEDs) 20,000 Nominal Lumens					<b>WH</b> =White				
PRV-XL=Prevail XL BAA-PRV-XL=Prevail XL BAA Compliant <sup>23</sup> TAA-PRV-XL=Prevail XL TAA Compliant <sup>23</sup>	C75=(4 LED) 26,100 Nominal Lumens C100=(4 LED) 31,000 Nominal Lumens C125=(4 LED) 36,000 Nominal Lumens C150=(6 LED) 41,100 Nominal Lumens C175=(6 LED) 48,600 Nominal Lumens									

BAA-PRV-XL=Prevail XL BAA Compliant <sup>23</sup> TAA-PRV-XL=Prevail XL TAA Compliant <sup>23</sup>	C105=(4 LED) 31,000 Nominal Lumens C125=(4 LED) 36,000 Nominal Lumens C150=(6 LED) 41,100 Nominal Lumens C175=(6 LED) 48,600 Nominal Lumens				
	Options (Add as Suffix)			Accesso	ries (Order Separately) <sup>17, 24</sup>
7030=70 CRI / 3000K CCT <sup>5</sup> 7050=70 CRI / 5000K CCT <sup>5</sup> HSS=House Side Shield <sup>6</sup> L90=Optics Rotated 90° Left R90=Optics Rotated 90° Left R90=Optics Rotated 90° Right 10K=10kV UL 1449 Fused Surge Protective Devic 20KS=20K MOV Surge Protective Device 20K=Series 20kV UL 1449 Surge Protective Devic Ha=50°C High Ambient Temperature <sup>7</sup> CC=Coastal Construction <sup>8</sup> PER=NEMA 3-PIN Twistlock Photocontrol Recept PER7=NEMA 7-PIN Twistlock Photocontrol Recept MS/DIM-L08=Dimming Motion and Daylight Sens MS/DIM-L20=Dimming Motion and Daylight Sens SPB1=Dimming Motion and Daylight Sensor, Blue SPB2=Dimming Motion and Daylight Sensor, Blue SPB2=Dimming Motion and Daylight Sensor, Blue SPB4=Dimming Motion and Daylight	tacle <sup>26</sup> tacle <sup>26</sup> tacle <sup>26</sup> totacle <sup>26</sup> sor, IR Remote Programmable, < 8' Mounting <sup>9,18</sup> sor, IR Remote Programmable, 8' - 20' Mounting sor, IR Remote Programmable, 21' - 40' Mounting tooth Programmable, < 8' Mounting <sup>9,11</sup> etooth Programmable, 22' - 20' Mounting <sup>9,13</sup> etooth Programmable, 21' - 40' Mounting <sup>9,14</sup> etooth Programmable, 21' - 40' Mounting <sup>9,15</sup> daylight, WAC Programmable, 7' - 15' Mounting totion and Daylight, WAC Programmable, 7' - 15' not and Daylight, WAC Programmable, 7' - 15' not and Daylight, WAC Programmable, 7' - 15' Nounting	g 9, 10 ng 9, 10 1, 9, 12, 13, 14 g 9, 12, 13, 14 Mounting 9, 12, 13, 14	PRVMA-XX=Mas PRVWM-XX=Wal PRVXLSA-XX=Six PRVXLWM-XX=M MA1010-XX=Sin MA1011-XX=Sin MA1011-XX=Sin MA1018-XX=Sin MA1018-XX=Sin MA1018-XX=Sin MA1018-INC OA/RA1013-Pho OA/RA1013-Pho OA/RA1013-Pho OA/RA1013-NEM OA/RA1027-NEM FSIR-100-Wireles SWPD4-XX=Wave 7'-15' Mounting SWPD5-XX=Wave 15'-40' Mounting	andard Arm Mounting tast Arm Mounting tall Mount Kit 15 lgle Tenon Adapter f 80° Tenon Adapter gle Tenon Adapter gle Tenon Adapter f 80° Tenon Adapter f 80° Tenon Adapter f 80° Tenon Adapte Side Shield, Front al Glare Shield, Front al Glare Shield, Front al Glare Shield, Side tocontrol Shorting (IA Photocontrol - 1.1 A Photocontrol - 3.1 A Photocontrol - 3.1 A Photocontrol - 3.1 A Photocontrol - 5.1 Chinx Sensor, Dimm 2, 13, 14, 21° Leinx Sensor, Dimm 12, 13, 14, 21° Leinx Sensor, Dimm 14, 13, 14, 21° Leinx Sensor, Dimm 14, 14,	ng Kit 15  Kit 15  or 3-1/2" O.D. Tenon  or 5-1/2" O.D. Tenon  or 2-3/8" O.D. Tenon  or 107 2-3/8" O.D. Tenon  /Back 19  19  2ap  20V  ulti-Tap 105-285V

ZD-SWPD4XX=WaveLinx, SR Driver, Dimming Motion and Daylight, WAC Programmable, 7' - 15' Mounting 9, 12, 13, 14
ZD-SWPD5XX=WaveLinx, SR Driver, Dimming Motion and Daylight, WAC Programmable, 15' - 40' Mounting 9, 12, 13, 14
(See Table Below)=LumenSafe Integrated Network Security Camera 15, 16 1. DesignLights Consortium® Qualified. Refer to <a href="https://www.designlights.org">www.designlights.org</a> Qualified Products List under Family Models for  $installation\ instructions\ IB 500002 EN\ and\ pole\ white\ paper\ WP 513001 EN\ for\ additional\ support\ information.$ 

6. Option will come factory-installed. House Side Shield not suitable with T5 distribution. Not available with PRV-C60 lumen 20. This tool enables adjustment to Motion Sensor (MS) parameters including high and low modes, sensitivity, time delay D. Uption with complete action y-installed. House sides shelled not suitable with 1s distribution. Not available with 1s distribution. Not available with PRV-C60 lumen package.

7. Not available with PRV-C60 lumen package. Not available with PRV-P-C25 lumen package.

8. Salt spray tested to over 5,000-hours per ASTM B117 with a scribe rating of 9 per ASTM D1654. Also achieves 7,000-hour rating per ASTM B117 with a scribe rating of 9 per ASTM D1654. Extended lead times may apply.

9. Controls system is not available in combination with a photocontrol receptacle (PER or PER7) or another controls systems (MS, ZD, ZW or LWR). Operates on 120-347V input voltages.

3. Only product configurations with these designated prefixes are built to be compliant with the Buy American Act of 1933 (BAA) or Trade Agreements Act of 1979 (TAA), respectively. Please refer to DOMESTIC PREFERENCES website for more information. 10. Utilizes the Wattstopper sensor FSP-211. Sensor color white unless specified otherwise via PDR. To field-configure, order mation. Components shipped separately may be separately analyzed under domestic preference requirements. Table. Field-configures via mobile application. See Controls section for details.

12. Sensor passive infrared (PIR) may be overly sensitive when operating below -20°C (-4°F).

13. For the device to be field-configurable, requires WAC Gateway components WAC-PoE and WPOE-120 in appropriate

25. Duravoit curvers reasone adverse adverse in the processor of the configurable and the processor of the processor of the configurable and the processor of the configurable and the processor of the processo

 $quantities. \ Only\ compatible\ with\ Wave Linx\ system\ and\ software\ and\ requires\ system\ components\ to\ be\ installed\ for\ operation.$ 

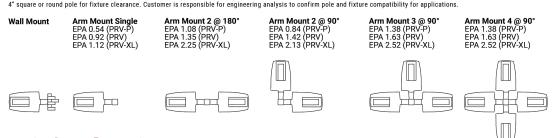
14. Replace XX with sensor color (WH, BZ, or BK). 15. Only available in PRV-XL configurations C75, C100, C125, C150, or C175. 2. Customer is responsible for engineering analysis to confirm pole and fixture compatibility for all applications. Refer to

16. Not available with 347V, 480V, DV, or HA options. Consult LumenSafe system product pages for additional details and 19. Must order one per optic/LED when ordering as a field-installable accessory (1, 2, 4, or 6

24. For DAY of Arranguments, Consult factory for further information.

25. DuraVolt drivers feature added protection from power quality issues such as loss of neutral, transients and voltage fluctua-

### Mounting Configurations and EPAs

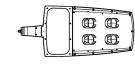


### **Optical Configurations**

PRV-C25/C40/C60 PRV-XL-C75/C100/C125 PRV-XL-C150/C175 (13,100/17,100/20,000 (26,100/31,000/36,300 Nominal Lumens) PRV-XL-C150/C175 (41,100/48,600 Nominal Lumens)











### Scottsdale® Legacy (CRUS)

LED Canopy Luminaire







114 -156

23 (10.4)



packages available.

HZ input.

Driver components are fully encased in

with IEC and FCC standards. 0-10 V

Die-cast aluminum, wet location rated

driver/electrical enclosure is elevated

potting for moisture resistance. Complies

dimming supplied standard with all drive

above canopy deck to prevent water entry,

heatsink ensuring cool operation of internal

provide easy "knock-out" connection of

primary wiring and acts as the primary

housing via one-piece molded silicone

Universal voltage power supply, 120-277

• -40°C to 55°C (-40°F to 131°F) ambient

performance data for specifics.)

Minimum 60,000 to 100,000 hours

of the installation location (see

Designed for lighter than air fuel

**Hazardous Location** 

performance data for specifics.)

VAC, 50/60 HZ and 347-480 VAC, 50/60

operating temperature. (Varies based on

depending upon the ambient temperature

applications. Product is suitable for Class 1

Divisions 2 only when properly installed per

LSI installation instructions. See Isicorp.com

for specific guidance. Not available on SLW.

lumen package and mounting style see

### **FEATURES & SPECIFICATIONS**

### Construction

Wattage Range

Weight lbs(kg)

• Features a ultra-slim 11/16" profile diecast housing, with flat clear or diffused tempered glass lens. Unit is water-resistant, sealed and IP66 rated. Integral designed heat sink does not trap dirt and grime, ensuring cool running performance over the life of the fixture.

• Standard color is white and is finished with LSI's DuraGrip® polyester powder coat process. DuraGrip withstands extreme weather changes without cracking or

• Luminaire assembly incorporates a pressure stabilizing vent breather to prevent seal fatigue and failure.

### **Optical System**

• Features an array of select, mid-power, high brightness, high efficiency LED; 3000K, 4000K, 5000K color temperature, 80 CRI (nominal).

 Choice of Symmetric or Asymmetric distribution. Asymmetric provides a wider distribution pattern. Optional symmetric with diffused lens also available. • Diffuse lens available as an option to soften

brightness of the luminaire. • Six Lumen Packages: 5,000, 9,000, 10,000, 13,000, 18,000 and 22,000 Lumens.

### **Electrical**

 High performance factory programmable driver features over-voltage, under voltage, short-circuit and over temperature protection with integral 6kV surge protection that meets IEEE C62.41.2 and ANSI C82.77-5 Location Category C Low

### standards. Additional field replaceable 10kV Installation

surge protection device meets a minimum Category C Low operation (per ANSI/

IEEE C62.41.2). Custom lumen and wattage • Installs in a 12" or 16" deck pan. Deck penetration consists of a 4" hole, simplifying installation and water sealing. Unit is designed to quickly retrofit into existing Scottsdale (4") hole as well as openings for Encore and Encore Top Access and to reconnect wiring for the SC/ECTA without having to relocate the conduit. • Retro panels are available for existing

Encores as well as kits for recessed and 2x2

Support brackets are provided standard, to

installations (see separate spec sheets).

### prevent sagging of deck. components for longer life. Seals to optical

• LSI LED fixtures carry a 5-year warranty (contact your LSI representative for extended warranty options.)

• UL and ETL listed to UL 1598, UL 8750 and other U.S. and International safety standards. Suitable for wet locations. • DesignLights Consortium® (DLC) qualified product. Not all versions of this product may be DLC qualified. Please check the DLC Qualified Products List at www.designlights. org/QPL to confirm which versions are

• Meets Buy American Act requirements. • IDA compliant with 3000K or lower color temperature.

Page 1/4 Rev. 12/10/21

### LSI Industries Inc. 10000 Alliance Rd. Cincinnati, OH 45242 • www.lsicorp.com (513) 372-3200 • ©2020 LSI Industries Inc. All Rights Reserved. Specifications subject to change without notice.



DELIVERED LUMENS

Scottsdale®	Legacy	LED	Canopy	Luminaire	(CRU
				Ва	ack to Quick

L ORDER EXAMPLE:	CRUS	SC	LED	SS	50	UE	WH

TYPICAL URDER EXAMPLE:	<u>CRUS</u>	SC LEL	) 88 50 UE	WHI			
Prefix	Distribution	Light Source	Drive Current	Color Temp	Input Voltage	Finish	Options
CRUS - LED Canopy Luminaire	SC - Symmetric AC¹ - Asymmetric	LED	SLW - 5,000 Lumens VLW - 9,000 Lumens LW - 11,000 Lumens SS - 13,000 Lumens HO - 19,000 Lumens VHO - 22,000 Lumens Custom Lumen Packages <sup>3</sup>	<b>50</b> - 5,000K <b>40</b> - 4,000K <b>30</b> - 3,000K	UE - Universal Voltage (120 - 277V) HV - High Voltage 347 - 480V	WHT - White BRZ - Bronze BLK - Black	<b>HL</b> <sup>2</sup> - Hazardous Location <b>DFL</b> - Diffuse Lens

2. Not available on SLW.	ances but not DLC listed.						
Accessory Ordering Information (Accessories a	are field installed)						
Description	Order Number	Description	Order Numbe				
Retrofit Panels - EC / ECTA / SCF to CRUS, for 16" Deck Panel	525946	Retrofit RIC Cover Panel Blank (no holes)	354702				
Retrofit Panels - ECTA / SCF to CRUS, for 12" Deck Panel	530281	Kit - Hole Plugs and Silicone (enough for 25 retrofits) <sup>1</sup>	1320540				
Retrofit 2x2 Cover Panel Blank (no holes)	357282						

AC distribution utilizes a reflector which alters the look from a standard SC distribution.

3. Custom lumen and wattage packages available consult factory. Values are within industry standard toler-

Description	Order Number	Description	Order Numbe
Retrofit Panels - EC / ECTA / SCF to CRUS, for 16" Deck Panel	525946	Retrofit RIC Cover Panel Blank (no holes)	354702
Retrofit Panels - ECTA / SCF to CRUS, for 12" Deck Panel	530281	Kit - Hole Plugs and Silicone (enough for 25 retrofits) <sup>1</sup>	1320540
Retrofit 2x2 Cover Panel Blank (no holes)	357282		
PERFORMANCE		1 - Consists of (25) 7/8" hole plugs, (100) 5/16" hole plugs and (1) tube of RTV	Back to Quick Li

		3000K CCT				4000K CCT		!			
Lumen Package	Distribution	Delivered Lumens	Efficacy	BUG Rating	Delivered Lumens	Efficacy	BUG Rating	Delivered Lumens	Efficacy	BUG Rating	Wattage
VIIO	SC	21301	140	B4-U0-G2	21835	144	B4-U0-G2	22697	150	B4-U0-G2	150
VH0	AC	17355	114	B3-U0-G3	17799	117	B3-U0-G3	18502	122	B3-U0-G3	152
110	SC	17889	143	B3-U0-G1	18346	146	B3-U0-G2	19071	152	B4-U0-G2	125
H0	AC	14582	116	B3-U0-G2	14955	119	B3-U0-G2	15546	124	B3-U0-G2	125
SS	SC	13113	141	B3-U0-G1	13449	144	B3-U0-G1	13980	150	B3-U0-G1	93
55	AC	11468	123	B3-U0-G2	11761	126	B3-U0-G2	12226	131	B3-U0-G2	93
110/	SC	10457	144	B3-U0-G1	10724	148	B3-U0-G1	11148	154	B3-U0-G1	70
LW	AC	9145	126	B2-U0-G2	9379	129	B2-U0-G2	9749	134	B2-U0-G2	73
1/11/4/	SC	8783	146	B3-U0-G1	9008	149	B3-U0-G1	9364	155	B3-U0-G1	00
VLW	AC	7681	127	B2-U0-G1	7878	131	B2-U0-G1	8189	136	B2-U0-G1	60
CIW	SC	5585	146	B2-U0-G1	5728	150	B2-U0-G1	5954	156	B2-U0-G1	20
SLW	AC	4884	128	B1-U0-G1	5009	131	B1-U0-G1	5207	136	B1-U0-G1	38

Lumen Package	Wattage	277V	347V	480\			
VH0	152	1.27	0.73	0.64	0.55	0.44	0.3
НО	124	1.03	0.6	0.52	0.45	0.36	0.20
SS	92	0.77	0.44	0.38	0.33	0.27	0.19
LW	72	0.6	0.35	0.3	0.26	0.21	0.18
VLW	60	0.5	0.29	0.25	0.22	0.17	0.13
SLW	38	0.32	0.18	0.16	0.14	0.11	0.08

VLW	60	0.25	0.22	0.17	0.1								
SLW	38	0.32	0.18	0.16	0.14	0.11	0.0						
Electrical data	at 25C (77F).	Actual watta	age may diff	er by +/-109	<b>%.</b>								
OPERATING TEMPERATURE													
LUMEN PACK	AGE	MOUNTING		Max									
VHO	N	letal/Wood Ca	пору	45 C									
VHO Metal/Wood Canopy 45 C													

	Ambient Temp C	Initial <sup>2</sup>	25k hr <sup>2</sup>	50k hr <sup>2</sup>	75k hr ³	100k hr					
	0 C	102%	97%	92%	88%	84%					
	10 C	102%	97%	92%	88%	84%					
	20 C	102%	97%	92%	88%	84%					
	25 C	102%	97%	92%	88%	84%					
Γ	30 C	102%	97%	92%	88%	84%					
	40 C	101%	95%	90%	85%	80%					
	50 C	101%	94%	89%	83%	78%					
Recommended Lumen Maintenance¹ CRUS SS											
	Ambient Temp C	Initial <sup>2</sup>	25k hr <sup>2</sup>	50k hr <sup>2</sup>	75k hr ³	100k hr					
ı	0 C	102%	97%	92%	88%	84%					
	0 C 10 C	102% 102%	97% 97%	92% 92%	88% 88%	84% 84%					
						• .,.					
	10 C	102%	97%	92%	88%	84%					
	10 C 20 C	102% 102%	97% 97%	92% 92%	88% 88%	84% 84%					
	10 C 20 C 25 C	102% 102% 102%	97% 97% 97%	92% 92% 92%	88% 88% 88%	84% 84% 84%					

1 - Lumen maintenance values at  $25\,^{\circ}\text{C}$  are calculated per TM-21 based on LM-80 data and in-situ 2 - In accordance with IESNA TM-21-11, Projected Values represent interpolated value based on NA LM-80-08 total test duration (in hours) for the device under testing (DUT) i.e. the packaged LED).

LSI Industries Inc. 10000 Alliance Rd. Cincinnati, OH 45242 • www.lsicorp.com
(513) 372-3200 • ©2020 LSI Industries Inc. All Rights Reserved. Specifications subject to change without notice.

### LITHONIA LIGHTING® Number

### **FEATURES & SPECIFICATIONS**

INTENDED USE — These specifications are for USA standards only. Square Straight Steel is a general purpose light pole for up to 39-foot mounting heights. This pole provides a robust yet cost effective option for mounting area lights and floodlights.

**CONSTRUCTION** — **Pole Shaft:** The pole shaft is of uniform dimension and wall thickness and is made of a weldable-grade, hot-rolled, commercial-quality steel tubing with a minimum yield of 55 KSI (11-gauge, .1196"), or 50 KSI (7-gauge, .1793"). Shaft is one-piece with a full-length longitudinal highfrequency electric resistance weld. Uniformly square in cross-section with flat sides, small corner radii and

excellent torsional qualities. Available shaft widths are 4", 5" and 6". **Pole Top:** A flush non-metalic black top cap is provided for all poles that will receive drilling patterns for side-mount luminaire arm assemblies or when ordered with PT option.

**Handhole:** A reinforced handhole with grounding provision is provided at 18" from the base on side A. Positioning the handhole lower may not be possible and requires engineering review; consult Tech Support-Outdoor for further information. Every handhole includes a cover and cover attachment hardware. The handhole has a nominal dimension of 2.5" x 5".

**Base Cover:** A durable ABS plastic two-piece full base cover, finished to match the pole, is provided with each pole assembly. Additional base cover options are available upon request.

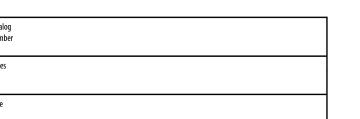
**Anchor Base/ Bolts:** Anchor base is fabricated from steel that meets ASTM A36 standards and can be altered to match existing foundations; consult factory for modifications. Anchor bolts are manufactured to ASTM F1554 Standards grade 55, (55 KSI minimum yield strength and tensile strength of 75-95 KSI). Top threaded portion (nominal 12") is hot-dipped galvanized per ASTM A-153.

**HARDWARE** – All structural fasteners are high-strength galvanized carbon steel. All non-structural fasteners are galvanized or zinc-plated carbon steel or stainless steel. **FINISH** – Extra durable standard powder-coat finishes include Dark Bronze, White, Black, Medium Bronze

and Natural Aluminum colors. Classic finishes include Sandstone, Charcoal Gray, Tennis Green, Bright Red and Steel Blue colors. Architectural Colors and Special Finishes are available by quote and include, but are not limited to Hot-dipped Galvanized, Paint over Hot-dipped Galvanized, RAL Colors, Custom Colors and Extended Warranty Finishes. Factory-applied primer paint finish is available for customer field-paint

**WARRANTY** — 1-year limited warranty. Complete warranty terms located at:  $\underline{www.acuitybrands.com/support/warranty/terms-and-conditions}$ 

**NOTE**: Actual performance may differ as a result of end-user environment and application. Specifications subject to change without notice.



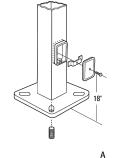
**Anchor Base Poles** 

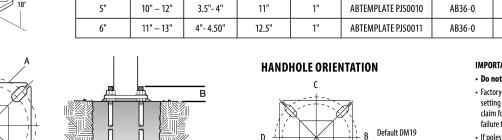
**SQUARE STRAIGHT STEEL** 

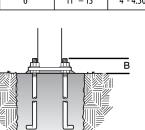
### **SSS** Square Straight Steel Poles

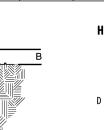
	Nominal	Pole Shaft Size					EPA (ft²) w	ith 1.3 gust			Bolt		Approximat
Catalog Number	Shaft Length (ft.)*	(Base in. x Top in. x ft.)	Wall thick (in)	Gauge	80 MPH	Max. weight	90 MPH	Max. weight	100 MPH	Max. weight	circle (in)	Bolt size (in. x in. x in.)	ship weigh (lbs.)
SSS 10 4C	10	4.0 x 10.0	0.1196	11	30.6	765	23.8	595	18.9	473	89	3/4 x 18 x 3	75
SSS 12 4C	12	4.0 x 12.0	0.1196	11	24.4	610	18.8	470	14.8	370	89	3/4 x 18 x 3	90
SSS 14 4C	14	4.0 x 14.0	0.1196	11	19.9	498	15.1	378	11.7	293	89	3/4 x 18 x 3	100
SSS 16 4C	16	4.0 x 16.0	0.1196	11	15.9	398	11.8	295	8.9	223	89	3/4 x 18 x 3	115
SSS 18 4C	18	4.0 x 18.0	0.1196	11	12.6	315	9.2	230	6.7	168	89	3/4 x 18 x 3	125
SSS 20 4C	20	4.0 x 20.0	0.1196	11	9.6	240	6.7	167	4.5	150	89	3/4 x 18 x 3	140
SSS 20 4G	20	4.0 x 20.0	0.1793	7	14	350	11	275	8	200	89	3/4 x 30 x 3	198
SSS 20 5C	20	5.0 x 20.0	0.1196	11	17.7	443	12.7	343	9.4	235	1012	1 x 36 x 4	185
SSS 20 5G	20	5.0 x 20.0	0.1793	7	28.1	703	21.4	535	16.2	405	1012	1 x 36 x 4	265
SSS 25 4C	25	4.0 x 25.0	0.1196	11	4.8	150	2.6	100	1	50	89	3/4 x 18 x 3	170
SSS 25 4G	25	4.0 x 25.0	0.1793	7	10.8	270	7.7	188	5.4	135	89	3/4 x 30 x 3	245
SSS 25 5C	25	5.0 x 25.0	0.1196	11	9.8	245	6.3	157	3.7	150	1012	1 x 36 x 4	225
SSS 25 5G	25	5.0 x 25.0	0.1793	7	18.5	463	13.3	333	9.5	238	1012	1 x 36 x 4	360
SSS 30 4G	30	4.0 x 30.0	0.1793	7	6.7	168	4.4	110	2.6	65	89	3/4 x 30 x 3	295
SSS 30 5C	30	5.0 x 30.0	0.1196	11	4.7	150	2	50			1012	1 x 36 x 4	265
SSS 30 5G	30	5.0 x 30.0	0.1793	7	10.7	267	6.7	167	3.9	100	1012	1 x 36 x 4	380
SSS 30 6G	30	6.0 x 30.0	0.1793	7	19	475	13.2	330	9	225	1113	1 x 36 x 4	520
SSS 35 5G	35	5.0 x 35.0	0.1793	7	5.9	150	2.5	100			1012	1 x 36 x 4	440
SSS 35 6G	35	6.0 x 35.0	0.1793	7	12.4	310	7.6	190	4.2	105	1113	1 x 36 x 4	540
SSS 39 6G	39	6.0 x 39.0	0.1793	7	7.2	180	3	75			1113	1 x 36 x 4	605

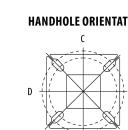
### BASE DETAIL



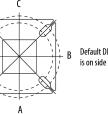








4"C 8" – 9" 3.25"- 3.75" 8"- 8.25" 0.75" ABTEMPLATE PJ50004 AB18-0 4"G 8" – 9" 3.38"- 3.75" 8"- 8.25" 0.875" ABTEMPLATE PJ50004 AB30-0



 Factory-supplied templates must be used when claim for incorrect anchorage placement due to failure to use Lithonia Lighting factory templates · If poles are stored outside, all protective wrapping must be removed immediately upon delivery to prevent finish damage. Lithonia Lighting is not responsible for the

POLE-SSS

N/A

**LITHONIA LIGHTING®** 

OUTDOOR: One Lithonia Way Conyers, GA 30012 Phone: 800-705-SERV (7378) www.lithonia.com

©1994-2021 Acuity Brands Lighting, Inc. All rights reserved. Rev. 01/22/21

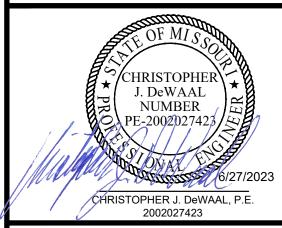


Chick-fil-A **5200 Buffington Road** Atlanta, Georgia 30349-2998



Together 1031-C W. 23rd Street Panama City, FL 32405 Ph: 850.563.1490

Plans Prepared By: CPH, LLC A Full Service A & E Firm



This item has been digitally signed and sealed by Christopher J DeWaal, PE. on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the

### signature must be verified on any electronic copies.

300 9 RFI

4

BUILDING TYPE / SIZE: RELEASE: **REVISION SCHEDULE** DESCRIPTION

CONSULTANT PROJECT # C291126 3/29/2023

produced for above named project may not be reproduced in any manner without express written or verbal consent from authorized project representatives. SITE LIGHTING CUT SHEETS

SHEET NUMBER

**ES-2.0**