


Planning Commission Staff Report

Meeting Date: November 22, 2021

From: Chris Dietz, Planner 

Location: 2272 Clarkson Rd.

Description: **P.Z. 09-2021 2272 Clarkson Road (SMS Group, LLC)**: A request for a change in zoning from an “NU” — Non-Urban District to a “PC”—Planned Commercial District for a 1.006-acre tract of land located at Clarkson Road and Wilson Avenue. (20T520103, 20T520394).

PROPOSAL SUMMARY

SMS Group, LLC is requesting a change in zoning from an “NU”—Non-Urban District to a “PC”—Planned Commercial District for a property just over an acre in size, located along the west side of Clarkson Road. The request includes changes to the permitted uses, setbacks and density requirements to accommodate the development of a small office building onsite.

A Public Hearing was held for this project at the October 11, 2021 Planning Commission meeting, during which two (2) issues were raised by the Commission. Both Staff and the applicant have provided a response to each of these issues, which will be discussed later in this report.

HISTORY OF SUBJECT SITE

The subject site was zoned “NU”—Non-Urban District prior to the City’s incorporation in 1988. Though the site was never developed, the subject site includes a tract of land that covers half of Old Wilson Road, which was vacated by the City of



Figure 1: Subject Site Aerial

Chesterfield in 2014. In 1993, a Conditional Use Permit was approved for the mortuary site to the north, which included the requirement for the site to provide cross access to this subject site as directed by the City of Chesterfield.

LAND USE AND ZONING

The land use and zoning for the properties surrounding the subject site are as follows:

Direction	Zoning	Land Use
North	“R1” —Residence District	Mortuary (C.U.P.)
South	“PC” —Planned Commercial District “R2” —Residence District	Retail Single-Family Residential
East	“PC” —Planned Commercial District	Retail
West	(City of Clarkson Valley)	Agriculture

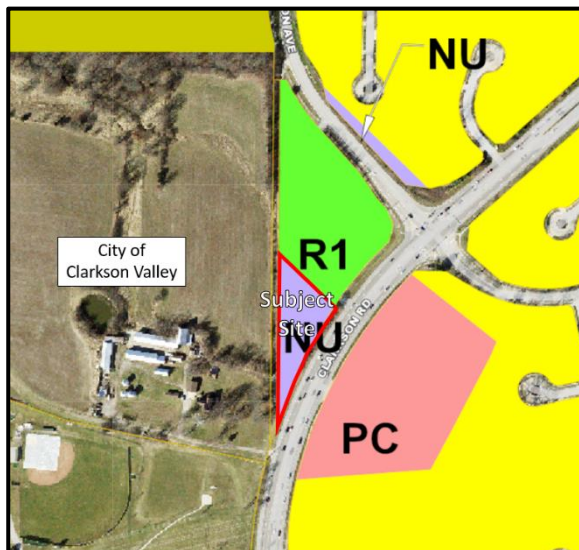


Figure 2: Zoning Map

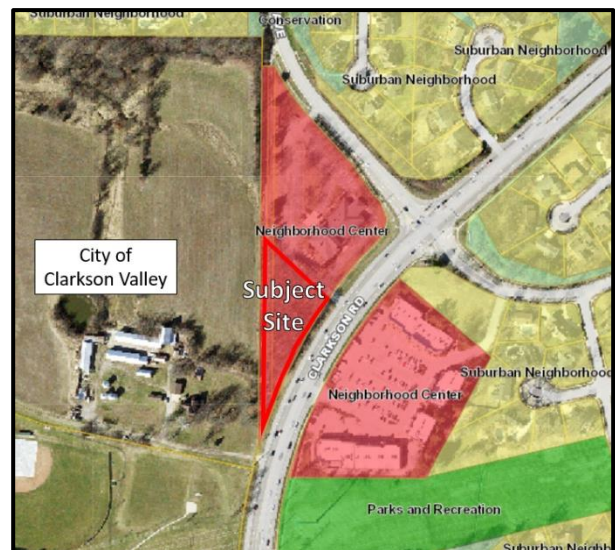


Figure 3: Land Use Map

COMPREHENSIVE PLAN

The subject site is located within the Neighborhood Center Character Area, as designated in the City’s Comprehensive Plan. This area is defined primarily by small-scale commercial centers that provide goods and services to surrounding neighborhoods. Due to the proximity to these neighborhoods, operations within this area are required to be low-intensity, unobtrusive and at a scale and design compatible with nearby residential development. These areas typically serve as transition areas between residential and non-residential uses and include pedestrian and bicycle accommodations for nearby residents.

PUBLIC HEARING

A Public Hearing was held for this project at the October 11, 2021 Planning Commission meeting, with two (2) issues being raised by Planning Commission. In light of these issues, Staff and the applicant have provided a response to each issue. These issues will be discussed in the section below.

ISSUES

Issue #1—Old Wilson Road

Issue: Planning Commission asked the Petitioner to provide additional information about the proposed use of the portion of the property in which the previous Old Wilson Road right-of-way was located, and whether they would consider pedestrian provisions along this area.

Applicant Response: The applicant has stated that they plan on accommodating pedestrian traffic along Clarkson rather than along Old Wilson Road that is no longer right-of-way, located on the property. The applicant lists the following reasons for this decision:

- Access to the abandoned road can only be gained through other private property.
- The former stretch of road terminates in an undeveloped lot, with no street connectivity.
- The road itself is in poor condition and has not been maintained, posing liability risk if used by pedestrians.
- Trash continuously collects along this area due to unauthorized use of this road by pedestrians.
- The sidewalk along Clarkson has more pedestrian visibility and more connectivity.
- A rest area has been provided along Clarkson Road on the Preliminary Development Plan to accommodate pedestrian traffic.
- The existing asphalt is proposed to be removed and reverted back to greenspace.

The applicant’s full response letter to this issue is found in the Planning Commission Packet.

Issue #2—Adjacent Zoning (Clarkson Valley)

Issue: Planning Commission requested additional information about the zoning designation for the adjacent property to the west, located in the Clarkson Valley municipality.

Staff Response: Staff has researched the City of Clarkson Valley’s City Code and confirms that the adjacent property, located at 2153 Wilson Avenue, is zoned “A” —Semi-Rural District. This zoning district allows single-family residential and other non-residential uses similar to those found in Chesterfield’s residential districts. The minimum lot size for single-family use is one (1) acre, with width of at least 125 feet. The maximum height for all buildings in this district is two (2) stories or thirty-five (35) feet. Setbacks for this district are provided in Table 1.

“A” Semi-Rural Setbacks	
Front	75’
Side	25’
Rear	50’

Table 1: Setbacks

STAFF ANALYSIS

The applicant’s request can be broken down into two (2) categories: Setbacks and Permitted Uses—both of which are described in detail below.

Setbacks

The current regulations for “NU” Districts do not define setbacks for parking specifically. The requested structure and parking setbacks within this “PC” District would be one and the same, with the front setbacks from Clarkson Road matching that of the adjacent mortuary site to the north, as indicated in the table below:

Current Structure Setbacks (“NU”)	Proposed Structure and Parking Setbacks (“PC”)	Adjacent Mortuary Site Structure and Parking Setbacks (C.U.P.)
Front (ROW): 50’ Side: 20’ Rear: 20’	Front (ROW): 30’ Side: 10’ Rear: 20’	Front (ROW): 30’ Side: 15’ (30’ Landscape Buffer—West) Rear: 15’

Table 2: Proposed Setbacks

Permitted Uses

The current permitted uses on site are predicated on the size of the property and do not include the permitted uses sought by the applicant. The current “NU” District and proposed “PC” District proposed Permitted Uses for this site are shown below:

Current Permitted Uses (“NU”)	Proposed Permitted Uses (“PC”)
<ul style="list-style-type: none"> • Churches • Commercial vegetable and flower gardening, as well as plant nurseries and greenhouses (no salesroom) • Dairy farming • Dwelling, single-family • Dwelling, single-family earth sheltered • Farming, including the cultivation and sale of any plant crops and domestic animals. • Forests, wildlife reservations, as well as conservation projects. • Golf courses, including practice driving tees on the same premises. (excluding mini-golf and independent practice driving tees). • Home Occupations • Hunting and fishing as well as propagation of wildlife of any kind. • Libraries (Public or Private) • Local public utility facilities 	<ul style="list-style-type: none"> • Office—General • Office—Dental • Office—Medical • Retail—Neighborhood (4,000 sq. ft. or less) • Retail—Community (4,000 – 25,000 sq. ft.) • Animal Grooming • Barber or Beauty Shop • Veterinary Clinic • Financial Institution—No Drive Through

Table 3: Proposed Uses

Density Requirements

While the narrative does not openly state the requested Open Space and Floor Area Ratio (FAR), the Preliminary Plan shows the Open Space set at 56% and FAR set at 0.17—both of which comply with UDC requirements (35% minimum and 0.55 maximum, respectively).

PRELIMINARY PLAN

The Preliminary Plan provided with this petition depicts a single office building and parking area on the property, with a shared access to Clarkson Road through the Mortuary site to the north. The Plan also includes an area for public art located in the parking area and a pedestrian bench located along Clarkson Road. Though not depicted, a sidewalk will be required along Clarkson Road once developed, as directed by the City and St. Louis County.

Staff has attached a copy of the draft Attachment “A” language, Narrative Statement, Issue Response Letter, Preliminary Development Plan, Survey, and Tree Stand Delineation for further consideration.

Attachments: Draft Attachment “A”
 Project Narrative
 Issue Response Letter
 Preliminary Development Plan
 Survey
 Tree Stand Delineation

ATTACHMENT A

All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified herein.

I. SPECIFIC CRITERIA

A. PERMITTED USES

1. The uses allowed in this “PC”—Planned Commercial District shall be:
 - a. Animal Grooming
 - b. Barber or Beauty Shop
 - c. Financial Institution—No Drive-Through
 - d. Office—Dental
 - e. Office—General
 - f. Office—Medical
 - g. Retail—Community
 - h. Retail—Neighborhood
 - i. Veterinary Clinic
2. Hours of Operation.
 - a. Uses “g” and “h” listed above are considered retail uses and retail sales, with respect to those uses, will be subject to hours of operation from 6:00 AM to 11:00 PM. Hours of operation for said uses may be expanded for Thanksgiving Day and the day after Thanksgiving upon review and approval of a Special Activities Permit, signed by the property owner and submitted to the City of Chesterfield at least seven (7) business days in advance of said holiday.
3. Telecommunication siting permits may be issued for wireless telecommunications facilities per the requirements of the City Code.

B. FLOOR AREA, HEIGHT, BUILDING AND PARKING STRUCTURE REQUIREMENTS

1. Floor Area
 - a. Total building floor area shall not exceed 7,300 square feet.
2. Height
 - a. The maximum height of the building, exclusive of roof screening, shall not exceed twenty (20) feet.
3. Building Requirements
 - a. A minimum of 35% open space is required for each lot within this development.

C. SETBACKS

1. Structure Setbacks

No building or structure, other than: a freestanding project identification sign, light standards, or flag poles will be located within the following setbacks:

 - a. Thirty (30) feet from the right-of-way containing Clarkson Road on the southeastern boundary of the Planned Commercial (“PC”) District.
 - b. Twenty (20) feet from the western boundary of the “PC” District.
 - c. Ten (10) feet from the northern boundary of the “PC” District.
2. Parking Setbacks

No parking stall, loading space, internal driveway, or roadway, except points of ingress or egress, will be located within the following setbacks:

 - a. Thirty (30) feet from the right-of-way containing Clarkson Road on the southeastern boundary of the Planned Commercial (“PC”) District.
 - b. Twenty (20) feet from the western boundary of the “PC” District.
 - c. Ten (10) feet from the northern boundary of the “PC” District.

D. PARKING AND LOADING REQUIREMENTS

1. Parking and loading spaces for this development will be as required in the City of Chesterfield Code.
2. Parking lots shall not be used as streets.

3. No construction related parking shall be permitted within right of way or on any existing roadways. All construction related parking shall be confined to the development.

E. LANDSCAPE AND TREE REQUIREMENTS

1. The development shall adhere to the Landscape and Tree Preservation Requirements of the City of Chesterfield Code.
2. A twenty-foot (20') landscape buffer shall be provided along the western border of this Planned Commercial District.
3. A thirty-foot (30') landscape buffer shall be provided along the right of way of Clarkson Road.

F. SIGN REQUIREMENTS

1. Signs shall be permitted in accordance with the regulations of the City of Chesterfield Code or a Sign Package may be submitted for the planned district. Sign Packages shall adhere to the City Code and are reviewed and approved by the City of Chesterfield Planning Commission.
2. Ornamental Entrance Monument construction, if proposed, shall be reviewed by the City of Chesterfield, and/or the St. Louis County Department of Highways and Traffic (or MoDOT), for sight distance considerations prior to installation or construction.

G. LIGHT REQUIREMENTS

Provide a lighting plan and cut sheet in accordance with the City of Chesterfield Code.

H. ARCHITECTURAL

1. The development shall adhere to the Architectural Review Standards of the City of Chesterfield Code.

I. ACCESS/ACCESS MANAGEMENT

1. Access to the development shall be as shown on the Preliminary Site Plan and adequate sight distance shall be provided, as directed by the City of Chesterfield, the Missouri Department of Transportation and St. Louis County Department of Transportation, as applicable.
2. If adequate sight distance cannot be provided at the access location(s), acquisition of right-of-way, reconstruction of pavement and other off-site improvements may be required to provide the required sight distance as

required by the City of Chesterfield and the agency in control of the right of way off which the access is proposed.

3. No vehicular access shall be allowed to Clarkson Road.

J. PUBLIC/PRIVATE ROAD IMPROVEMENTS, INCLUDING PEDESTRIAN CIRCULATION

1. Any request to install a gate at the entrance to this development must be approved by the City of Chesterfield and the agency in control of the right of way off of which the entrance is constructed. No gate installation will be permitted on public right of way.
2. If a gate is installed on a street in a development, the streets within the development or that portion of the development that is gated shall be private and remain private forever.
3. Provide street connections to the adjoining properties as directed by the City of Chesterfield. Stub street signage, in conformance with Section 405.04.090 of the Unified Development Code of the City of Chesterfield, shall be posted within 30 days of the street pavement being placed
4. Provide a 5 foot wide sidewalk, conforming to ADA standards, along Clarkson Road. The sidewalk shall provide for future connectivity to adjacent developments and/or roadway projects. The sidewalk may be located within right-of-way controlled by another agency, if permitted by that agency or on private property within a 6 foot wide sidewalk, maintenance and utility easement dedicated to the City of Chesterfield.
5. Obtain approvals from the City of Chesterfield and the Missouri Department of Transportation and other entities as necessary for locations of proposed curb cuts and access points, areas of new dedication, and roadway improvements.
6. Additional right-of-way and road improvements shall be provided, as required by the Missouri Department of Transportation and the City of Chesterfield.
7. Any work within MoDOT's right of way will require a MoDOT permit.
8. All proposed work in MoDOT right of way must comply with MoDOT standards, specifications, conform to MoDOT's Access Management Guidelines with detailed construction plans being received and approved by MoDOT.
9. The Missouri Department of Transportation has not approved this development proposal and its impacts to our roads and right-of-way. The developer should comply with all requirements of MoDOT.

10. All sidewalk and pedestrian facilities within MoDOT right of way must meet the ADA Standards.
11. Any utility work on MoDOT right of way will require a separate MoDOT permit.

K. TRAFFIC STUDY

1. Provide a traffic study as directed by the City of Chesterfield and/or Missouri Department of Transportation. The scope of the study shall include internal and external circulation and may be limited to site specific impacts, such as the need for additional lanes, entrance configuration, geometrics, sight distance, traffic signal modifications or other improvements required, as long as the density of the proposed development falls within the parameters of the City's traffic model. Should the density be other than the density assumed in the model, regional issues shall be addressed as directed by the City of Chesterfield.

L. POWER OF REVIEW

The development shall adhere to the Power of Review Requirements of the City of Chesterfield Code.

M. STORM WATER

1. The site shall provide for the positive drainage of storm water and it shall be discharged at an adequate natural discharge point or an adequate piped system.
2. Detention/retention and channel protection measures are to be provided in each watershed as required by the City of Chesterfield and the Metropolitan St. Louis Sewer District. The storm water management facilities shall be operational prior to paving of any driveways or parking areas in non-residential developments or issuance of building permits exceeding sixty percent (60%) of the approved dwelling units in each plat, watershed or phase of residential developments. The location and types of storm water management facilities shall be identified on all Site Development Plans.
3. Emergency overflow drainage ways to accommodate runoff from the 100-year storm event shall be provided for all storm sewers, as directed by the City of Chesterfield.
4. Offsite storm water shall be picked up and piped to an adequate natural discharge point. Such bypass systems must be adequately designed
5. Locations of site features such as lakes and detention ponds must be approved by the City of Chesterfield and the Metropolitan Saint Louis Sewer District.

6. Formal MSD review, approval, and permits are required.
7. Public sanitary sewers must be extended to serve this lot.
8. Offsite easements may be required.
9. Easements may be required to allow for future public sewer extensions to adjacent tracts.
10. The project is in the Caulks Creek Service area and is subject to the Caulks Creek Surcharge.
11. Post-construction BMP's will be required for disturbed area of one acre or greater.
12. The site is considered new development; if BMPs are required, volume reducing BMPs will be required.
13. Flood protection volume may be required based on site differential. Channel protection volume will be required for this project if flood protection volume is required.
14. Offsite drainage areas should be routed around the detention basin.
15. New encroachments will not be allowed.
16. MSD may require approval from the City of Chesterfield prior to issuing plan approval.

N. SANITARY SEWER

Sanitary sewers shall be as approved by the City of Chesterfield and the Metropolitan St. Louis Sewer District.

O. GEOTECHNICAL REPORT

Prior to Site Development Plan approval, the developer shall provide a geotechnical report, prepared by a registered professional engineer licensed to practice in the State of Missouri, as directed by the City of Chesterfield. The report shall verify the suitability of grading and proposed improvements with soil and geologic conditions and address the existence of any potential sinkhole, ponds, dams, septic fields, etc., and recommendations for treatment. A statement of compliance, signed and sealed by the geotechnical engineer preparing the report, shall be included on all Site Development Plans and Improvement Plans.

P. MISCELLANEOUS

1. Public art shall be provided as shown on the Preliminary Development Plan.
2. All utilities will be installed underground.
3. An opportunity for recycling will be provided. All provisions of Chapter 25, Article VII, and Section 25-122 thru Section 25-126 of the City Code shall be required where applicable.
4. Prior to record plat approval, the developer shall cause, at his expense and prior to the recording of any plat, the reestablishment, restoration or appropriate witnessing of all Corners of the United States Public Land Survey located within, or which define or lie upon, the out boundaries of the subject tract in accordance with the Missouri Minimum Standards relating to the preservation and maintenance of the United States Public Land Survey Corners, as necessary.

II. GENERAL CRITERIA

A. SITE DEVELOPMENT PLAN SUBMITTAL REQUIREMENTS

The Site Development Plan shall include, but not be limited to, the following:

1. Location map, north arrow, and plan scale. The scale shall be no greater than one (1) inch equals one hundred (100) feet.
2. Outboundary plat and legal description of property.
3. Density calculations.
4. Parking calculations. Including calculation for all off street parking spaces, required and proposed, and the number, size and location for handicap designed.
5. Provide open space percentage for overall development including separate percentage for each lot on the plan.
6. Provide Floor Area Ratio (F.A.R.).
7. A note indicating all utilities will be installed underground.
8. A note indicating signage approval is a separate process.
9. Depict the location of all buildings, size, including height and distance from adjacent property lines, and proposed use.

10. Specific structure and parking setbacks along all roadways and property lines.
11. Indicate location of all existing and proposed freestanding monument signs.
12. Zoning district lines, subdivision name, lot number, dimensions, and area, and zoning of adjacent parcels where different than site.
13. Floodplain boundaries.
14. Depict existing and proposed improvements within 150 feet of the site as directed. Improvements include, but are not limited to, roadways, driveways and walkways adjacent to and across the street from the site, significant natural features, such as wooded areas and rock formations, and other karst features that are to remain or be removed.
15. Depict all existing and proposed easements and rights-of-way within 150 feet of the site and all existing or proposed off-site easements and rights-of-way required for proposed improvements.
16. Indicate the location of the proposed storm sewers, detention basins, sanitary sewers and connection(s) to the existing systems.
17. Depict existing and proposed contours at intervals of not more than one (1) foot, and extending 150 feet beyond the limits of the site as directed.
18. Address trees and landscaping in accordance with the City of Chesterfield Code.
19. Comply with all preliminary plat requirements of the City of Chesterfield Subdivision Regulations per the City of Chesterfield Code.
20. Signed and sealed in conformance with the State of Missouri Department of Economic Development, Division of Professional Registration, Missouri Board for Architects, Professional Engineers and Land Surveyors requirements.
21. Provide comments/approvals from the appropriate Fire District, Monarch Levee District, Spirit of St. Louis Airport, Metropolitan St. Louis Sewer District (MSD) and the Missouri Department of Transportation.
22. Compliance with Sky Exposure Plane.
23. Compliance with the current Metropolitan Sewer District Site Guidance as adopted by the City of Chesterfield.

III. TRUST FUND CONTRIBUTION

The developer shall be required to contribute a Traffic Generation Assessment (TGA) to the Chesterfield Village Road Trust Fund (No. 554). Traffic generation assessment contributions shall be deposited with St. Louis County prior to the issuance of building permits. If development phasing is anticipated, the developer shall provide the Traffic Generation Assessment contribution prior to the issuance of building permits for each phase of development. Funds shall be payable to Treasurer, Saint Louis County.

A. ROADS

This TGA contribution shall not exceed an amount established by multiplying the required parking spaces by the following rate schedule:

<u>Type of Development</u>	<u>Required Contribution</u>
General Office	\$777.11/parking space
General Retail	\$2,331.45/parking space
Medical Office	\$2,331.45/parking space
Loading Space	\$3,815.12/parking space

(Parking spaces as required by the City of Chesterfield Code.)

If types of development differ from those listed, St. Louis County Department of Highways and Traffic will provide rates.

If a portion of the improvements required herein are needed to provide for the safety of the traveling public, their completion as a part of this development is mandatory.

Allowable credits for required roadway improvements will be awarded as directed by the Saint Louis County Department of Transportation and the City of Chesterfield. Sidewalk construction and utility relocation, among other items, are not considered allowable credits.

As this development is located within a trust fund area established by Saint Louis County, any portion of the traffic generation assessment contribution which remains following completion of road improvements required by the development shall be retained in the appropriate trust fund.

Traffic Generation Assessment contributions shall be deposited with Saint Louis County Department of Transportation. The deposit shall be made prior to the issuance of a Special Use Permit (S.U.P.) by Saint Louis County Department of Transportation or prior to the issuance of building permits in the case where no Special Use Permit is required.

The amount of all required contributions, if not submitted by January 1, 2022, shall be adjusted on that date and on the first day of January in each succeeding year thereafter in accordance with the construction cost index as determined by the Saint Louis County Department of Transportation.

Trust Fund contributions shall be deposited with St. Louis County in the form of a cash escrow prior to the issuance of building permits.

IV. RECORDING

Within sixty (60) days of approval of any development plan by the City of Chesterfield, the approved Plan will be recorded with the St. Louis County Recorder of Deeds. Failure to do so will result in the expiration of approval of said plan and require re-approval of a plan by the Planning Commission.

V. ENFORCEMENT

- A.** The City of Chesterfield, Missouri will enforce the conditions of this ordinance in accordance with the Plan approved by the City of Chesterfield and the terms of this Attachment A.
- B.** Failure to comply with any or all the conditions of this ordinance will be adequate cause for revocation of approvals/permits by reviewing Departments and Commissions.
- C.** Non-compliance with the specific requirements and conditions set forth in this Ordinance and its attached conditions or other Ordinances of the City of Chesterfield shall constitute an ordinance violation, subject, but not limited to, the penalty provisions as set forth in the City of Chesterfield Code.
- D.** Waiver of Notice of Violation per the City of Chesterfield Code.
- E.** This document shall be read as a whole and any inconsistency to be integrated to carry out the overall intent of this Attachment A.

Zoning Change Request
2272 Clarkson Rd Chesterfield MO 63017

We are requesting a change in zoning for 2272 Clarkson Rd from the current NU zoning to a PC District. This zoning request aligns the zoning with the most recent Comprehensive Plan for the City of Chesterfield. Below are the detailed parameters of our request:

Setbacks

- Front Building and Parking Setback (Southeast) along Clarkson Rd – 30'
- Rear Building and Parking Setback along Old Wilson Rd (West along private/vacated drive) – 20'
- Side yard Building and Parking Setback (Northeast along Bucholz Mortuary) – 10'

Uses

- Office
 - General
 - Dental
 - Medical
- Retail
 - Neighborhood
 - Community
- Service
 - Animal Grooming
 - Barber or Beauty Shop
 - Veterinary Clinic
 - Financial Institution – No Drive Through

Additional Features

- The proposed building height will not exceed 20' with a proposed area of +/-7300 sq ft
- We have provided for an art exhibit area in the center of the parking lot (island). Detail will be provided with the landscape plan.
- We have provided for a public use area in the Southern corner. We plan to create a sitting area for pedestrians, on foot or bike. Detail will be provided with the landscape plan.
- We will work to have a building design that is interesting and aligns with the spirit of the area.

Response to Item raised at PZ Meeting
P.Z. 09-2021—2272 Clarkson Rd. Change of Zoning

Issues Raised in the Public Hearing:

The following issue was raised during the Public Hearing that will require a formal response from the applicant prior to the next Planning Committee agenda on which this project may be reviewed:

1. Provide information about the proposed use of the portion of the property in which the previous Old Wilson Road right-of-way was located, and provide a statement on whether the applicant would consider pedestrian provisions along this area.

Response:

We thank you for your question and have evaluated various options with regard to maintaining some pedestrian walk way through the abandoned ROW “behind” the property located at 2272 Clarkson Rd. We feel that due to the fact that the ROW has been abandoned and the roadway is no longer being maintained that it would make more sense to move the pedestrian traffic to the sidewalk that will be located along Wilson Ave to Clarkson Rd, which will be extended along the Clarkson Rd frontage of the subject site. The main reasons for this response are:

- The access to the abandoned ROW behind the subject property can only be accessed through private land both to the north and south
- This former stretch of road terminates in a grassy lot owner by another party
- The asphalt has seriously deteriorated and poses possible risk and liability due to the potholes and chunks along that stretch.
- The “use” of this property has resulted in a large accumulation of trash that we are continually picking up (soda bottles, beer cans, paper trash, etc).
- From a safety standpoint it would be advisable to direct pedestrian traffic to the sidewalk along Clarkson Rd for better pedestrian visibility and proper cross access.
- We have provided, in our original preliminary site plan, for a pedestrian rest area along the southern end of the subject property.
- We intend to remove the remaining asphalt and return this area to green space.

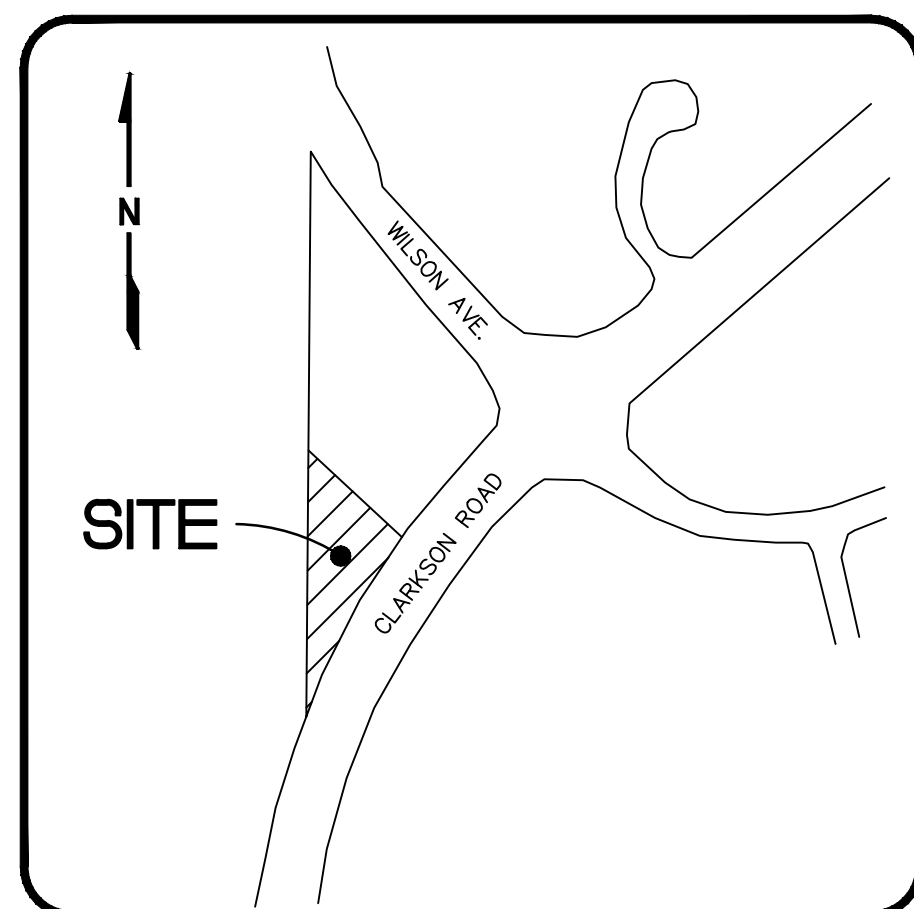
Thank you again for the thought behind this and the opportunity to think through the possibilities and respond.

Sincerely,

Sean Sortor
SMS Group LLC

LEGEND

DESCRIPTION	SYMBOL
EXISTING MAJOR CONTOUR	---500---
EXISTING MINOR CONTOUR	---502---
PROPOSED MAJOR CONTOUR	—504—
PROPOSED MINOR CONTOUR	---502---
PROPOSED SPOT ELEVATION	+502.00
EXISTING SANITARY SEWER	---●---
EXISTING STORM SEWER	---□---
PROPOSED SANITARY SEWER	---●---
PROPOSED STORM SEWER	---□---
EXISTING WATERLINE	—W—
EXISTING FIRE HYDRANT	⦿
EXISTING GAS LINE	—G—
EXISTING OVERHEAD UTILITY	—OE—
USE IN PLACE	(U.I.P.)
ADJUST TO GRADE	(A.T.G.)
TO BE REMOVED	(T.B.R.)
TO BE REMOVED AND REPLACED	(T.B.R.&R.)
TO BE REMOVED AND RELOCATED	(T.B.R.&REL.)



LOCATION MAP
N.T.S.

PRELIMINARY DEVELOPMENT PLAN
2272 CLARKSON ROAD
CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI

PROPERTY DATA

LOCATOR NO. : 201520103
 ADDRESS : 2272 CLARKSON ROAD
 CHESTERFIELD, MO 63017
 OWNER : SMS GROUP, LLC
 AREA OF TRACT : 43,833 S.F. = 1.01 Ac.±
 PRESENT ZONING : NU - NON URBAN
 PROPOSED ZONING : PC - PLANNED COMMERCIAL
 PRESENT USAGE : VACANT
 PROPOSED USAGE : OFFICE BUILDING
 SCHOOL DISTRICT : ROCKWOOD
 FIRE DISTRICT : MONARCH
 WATERSHED(S) : MISSOURI RIVER
 FIRM PANEL : 29189C0164K
 UTILITIES : MISSOURI-AMERICAN WATER CO.
 METRO, ST. LOUIS SEWER DIST.
 LACLEDE GAS COMPANY
 AT&T TELEPHONE COMPANY
 AMEREN UE

PROPERTY LEGAL DESCRIPTION

A TRACT OF LAND IN THE NORTH HALF OF SECTION 21, TOWNSHIP 45 NORTH, RANGE 4 EAST, IN THE COUNTY OF ST. LOUIS, MISSOURI, AS DESCRIBED IN DEED RECORDED IN DEED BOOK 20715 PAGE 1964 AND DEED RECORDED IN DEED BOOK 21137 PAGE 1282, AND BEING FURTHER DESCRIBED AS: BEGINNING AT THE SOUTHERNMOST CORNER OF PROPERTY CONVEYED TO BUCHHOLZ MORTUARIES INC. IN DEED BOOK 9743 PAGE 810 OF THE ST. LOUIS COUNTY RECORDS; THENCE NORTH 44 DEGREES 29 MINUTES 13 SECONDS WEST 218.87 FEET TO A POINT; THENCE NORTH 89 DEGREES 47 MINUTES 20 SECONDS WEST 20.00 FEET TO A POINT; THENCE SOUTH 0 DEGREES 12 MINUTES 40 SECONDS WEST 520.24 FEET TO A POINT; THENCE ALONG A CURVE TO THE RIGHT HAVING A RADIUS OF 1517.69 FEET AND A LENGTH OF 113.66 FEET (CHORD BEARING NORTH 21 DEGREES 38 MINUTES 34 SECONDS EAST 113.63 FEET) TO A POINT; THENCE NORTH 24 DEGREES 59 MINUTES 08 SECONDS EAST 209.28 FEET; THENCE ALONG A CURVE TO THE RIGHT HAVING A RADIUS OF 1527.69 FEET AND A LENGTH OF 82.13 FEET (CHORD BEARING NORTH 33 DEGREES 12 MINUTES 40 SECONDS EAST 82.12 FEET) TO THE POINT OF BEGINNING.

SMS Group LLC
1717 Wilson Avenue
Chesterfield, MO 63005

Vance Engineering, Inc.
10537 Lackland Road
St. Louis, MO 63114
P: 314.427.1800

2272 CLARKSON ROAD

PRELIMINARY DEVELOPMENT PLAN

PRELIMINARY

MICHAEL CLAY VANCE, P.E.
ENGINEER LIC E-25616

REVISOR
8/26/21 CITY COMMENTS
9/14/21 OMIT MON SIGN
10/27/21 "PRELIMINARY"

21017
07/28/21
1/1
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SETBACK REQUIREMENTS

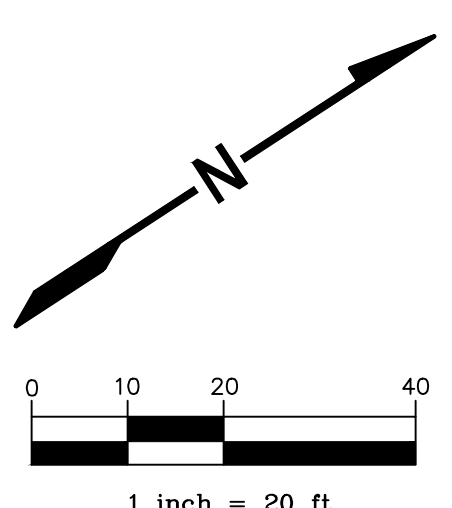
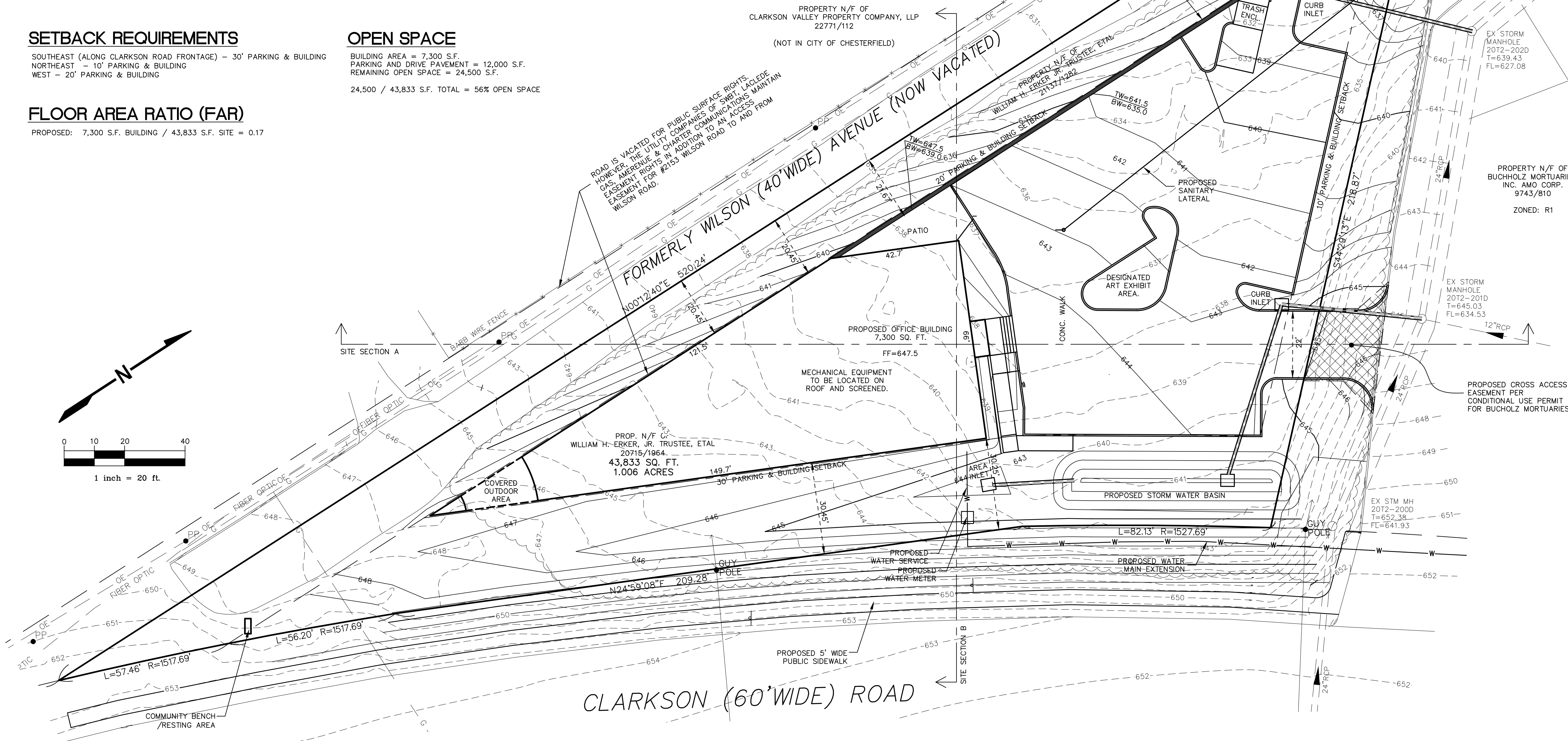
SOUTHEAST (ALONG CLARKSON ROAD FRONTAGE) - 30' PARKING & BUILDING
 NORTHEAST - 10' PARKING & BUILDING
 WEST - 20' PARKING & BUILDING

OPEN SPACE

BUILDING AREA = 7,300 S.F.
 PARKING AND DRIVE PAVEMENT = 12,000 S.F.
 REMAINING OPEN SPACE = 24,500 S.F.
 24,500 / 43,833 S.F. TOTAL = 56% OPEN SPACE

FLOOR AREA RATIO (FAR)

PROPOSED: 7,300 S.F. BUILDING / 43,833 S.F. SITE = 0.17



SMS GROUP, LLC, THE OWNER(S) OF THE PROPERTY SHOWN ON THIS PLAN FOR AND IN CONSIDERATION OF BEING GRANTED APPROVAL OF SAID PLAN TO DEVELOP PROPERTY UNDER THE PROVISIONS OF SECTION 03 OF CITY OF CHESTERFIELD UNIFIED DEVELOPMENT CODE, DO HEREBY AGREE AND DECLARE THAT SAID PROPERTY FROM THE DATE OF RECORDING THIS PLAN SHALL BE DEVELOPED ONLY AS SHOWN THEREON, UNLESS SAID PLAN IS AMENDED BY THE CITY OF CHESTERFIELD, OR VOIDED OR VACATED BY ORDER OF ORDINANCE OF THE CITY OF CHESTERFIELD COUNCIL.

SEAN M SORTOR
 STATE OF MISSOURI }
 COUNTY OF ST. LOUIS } ss.

ON THIS _____ DAY OF _____ A.D., 20____ BEFORE ME PERSONALLY APPEARED SEAN M. SORTOR, TO ME KNOWN, WHO, BEING BY ME SWORN IN, DID SAY THAT HE IS THE MANAGING MEMBER OF SMS GROUP, LLC A LIMITED LIABILITY CORPORATION IN THE STATE OF _____ AND THAT THE SEAL AFFIXED TO THE FOREGOING INSTRUMENTS IS THE CORPORATE SEAL OF SAID CORPORATION, AND THAT SAID INSTRUMENT WAS SIGNED ON BEHALF OF SAID LIMITED LIABILITY CORPORATION BY AUTHORITY OF ITS MEMBERS, AND THE SAID SEAN M. SORTOR ACKNOWLEDGED SAID INSTRUMENT TO BE THE FREE ACT AND DEED OF SAID LIMITED LIABILITY CORPORATION.

IN TESTIMONY WHEREOF, I HAVE HERETO SET MY HAND AND AFFIXED MY NOTARIAL SEAL AT MY OFFICE IN ST. LOUIS COUNTY, MISSOURI, THE DAY AND YEAR LAST ABOVE WRITTEN.

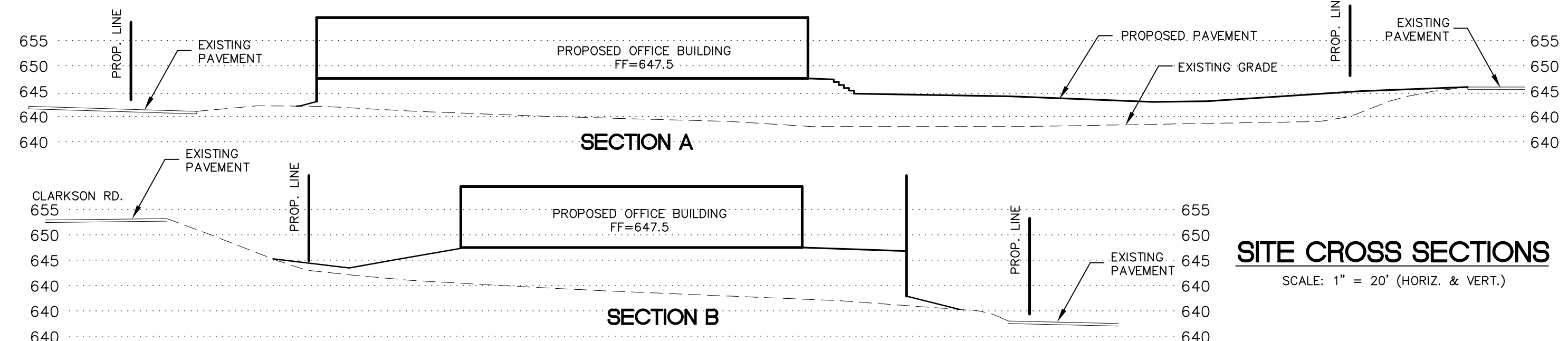
MY TERM EXPIRES _____

(NOTARY PUBLIC)

THIS SITE DEVELOPMENT PLAN WAS APPROVED BY THE CITY OF CHESTERFIELD PLANNING COMMISSION AND DULY VERIFIED ON THE _____ DAY OF _____ 20____ BY THE CHAIRPERSON OF SAID COMMISSION, AUTHORIZING THE RECORDING OF THIS SITE DEVELOPMENT PLAN PURSUANT TO CHESTERFIELD ORDINANCE NUMBER 200, AS ATTESTED TO BY THE DIRECTOR OF PLANNING AND DEVELOPMENT SERVICES AND THE CITY CLERK.

JUSTIN WYSE, AICP
 DIRECTOR OF PLANNING AND DEVELOPMENT SERVICES
 CITY OF CHESTERFIELD, MISSOURI

VICKIE HASS,
 CITY CLERK
 CITY OF CHESTERFIELD, MISSOURI

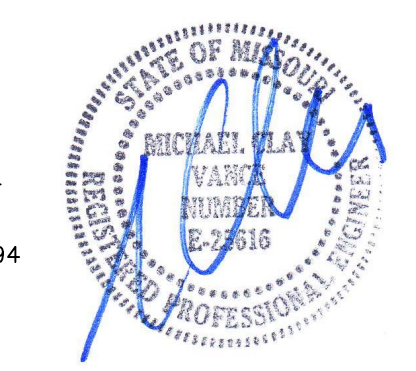


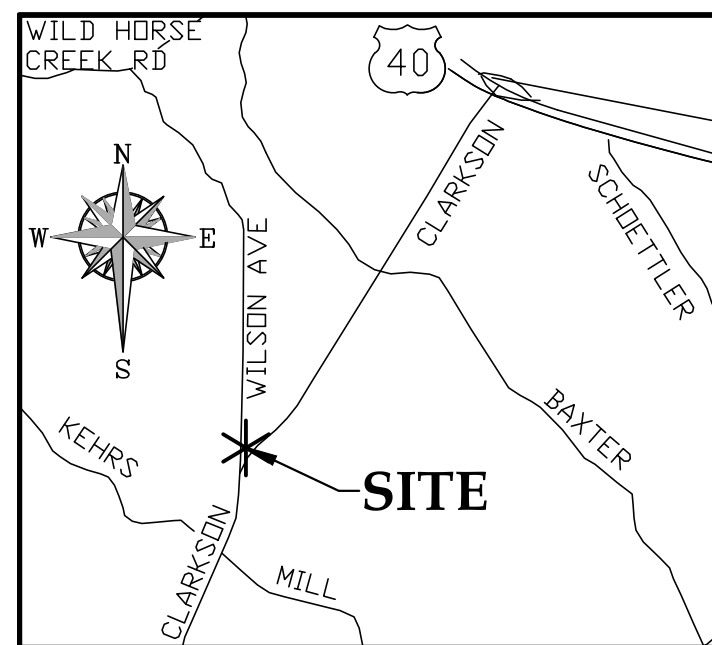
SITE CROSS SECTIONS
SCALE: 1" = 20' (HORIZ. & VERT.)

THIS PLAN WAS PREPARED USING RECORD/AVAILABLE SURVEYS, PROPERTY DESCRIPTIONS, TOPOGRAPHIC INFORMATION, UTILITY MAPS/MARKINGS, AERIAL PHOTOS, ETC. AS SUCH, THE DESIGN OF IMPROVEMENTS SHOWN HEREON IS TO BE CONSIDERED STRICTLY PRELIMINARY AND SUBJECT TO REVISION AS A RESULT OF ACTUAL BOUNDARY SURVEYS, FINAL ENGINEERING DESIGN AND AGENCY(S) REVIEW/APPROVAL.

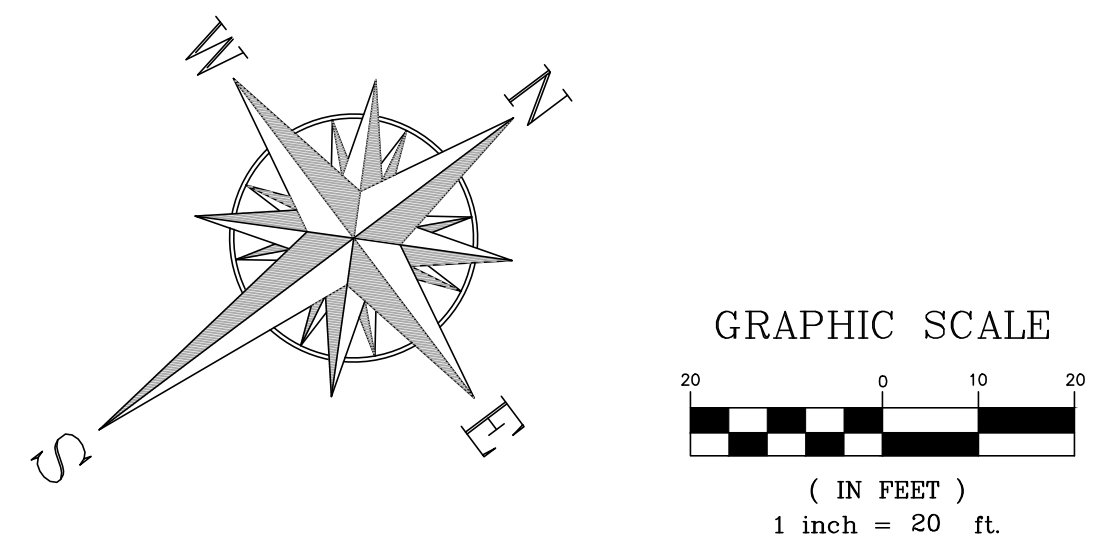
I HEREBY CERTIFY THAT I HAVE PREPARED OR DIRECTED THE PREPARATION OF THIS PLAN BASED ON THIS INFORMATION, IT IS TO BE CONSIDERED STRICTLY PRELIMINARY, AND IS NOT TO BE USED FOR CONSTRUCTION.

Michael Clay Vance
 10/27/21
 MICHAEL CLAY VANCE, PE
 E-25616
 VANCE ENGINEERING, INC. AUTHORITY NO. 2003022194





LOCATION MAP
NOT TO SCALE



BOUNDARY & TOPOGRAPHIC SURVEY FOR #2272 CLARKSON ROAD

A TRACT OF LAND BEING IN THE NORTH HALF OF
SECTION 21, TOWNSHIP 45 NORTH, RANGE 4 EAST
CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI

11/02/2021

- GENERAL NOTES:**
- The basis of the bearing system is the right-of-way line of vacated Wilson (40' W.) Avenue per Deed Book 20715, Page 1964. Said bearing is north 00 degrees, 12 minutes, 40 seconds east.
 - This is an urban survey.
 - () denotes record information.
 - Title Commitment provided is per First American Title Insurance Company with a commitment date of December 21, 2020 and a commitment number of 210066. See Schedule "B", Section II for the following survey related items:

SURVEYOR'S STATEMENT

This is to certify that we have, by the order of Sean Sorter, during June 2019, executed a Boundary & Topographic Survey of a tract of land being in the north half of Section 21, Township 45 North, Range 4 East, St. Louis County, Missouri, in accordance with the current minimum standards for property boundary surveys of the Missouri Department of Agriculture, Land Survey office, and that result of said survey is shown on this plat. We certify that the above plat was made from an actual survey according to the records available and recorded.



Michael A. Sater
Michael A. Sater
Missouri P.L.S. #2001001915

PROPERTY N/F OF
CLARKSON VALLEY PROPERTY COMPANY, LLP
22771/112

- LEGEND**
- FOUND CONCRETE MONUMENT
 - FOUND IRON PIPE/ROD
 - ⊙ SET IRON ROD W/ CAP
 - ⊕ FOUND/SET CROSS
 - GUY WIRE
 - PP POWER POLE
 - ☆ LIGHT STANDARD
 - ⊗ YARD LIGHT
 - ⚡ FIRE HYDRANT
 - WM WATER METER
 - WV WATER VALVE
 - SC SANITARY CLEANOUT
 - GV GAS VALVE
 - EM ELECTRIC METER
 - STREET SIGN
 - FENCE
 - ✕ MAIL BOX
 - 🌲 PINE TREE
 - 🌳 DECIDUOUS
 - 🌿 BUSHES/HEDGES
 - 🌱 BUSH
 - OE OVERHEAD ELECTRIC
 - 6"W. WATER LINE
 - 2"G. GAS LINE
 - FO FIBER OPTIC
 - UE UNDERGROUND ELECTRIC
 - EXIST. STORM SEWER
 - EXIST. SANITARY/COMBINED SEWER
 - CONC. ASPH.

This document contains the ordinance which vacated the portion of Wilson road adjacent to the subject site. Also this document creates easement in the same area for utilities and access for the property owner at #2153 Wilson Road. The utilities with easement rights include Southwestern Bell Telephone, AmerenUE, Charter Communications, and LaCade gas Company.

The easement in Book 1095 Page 381 may affect this site on the southern end but appears to have been reserved for transmission lines that were built farther south, and therefore may be considered extinguished for non use.

The easement in Book 3307 Page 93 also appear to be for the electric transmission lines built farther to the south of this tract.

The easement in Book 6324 Page 2336 also appear to be for the electric transmission lines built farther to the south of this tract.

item 15) easements granted to Union Electric Company, according to instruments recorded in Book 1095 Page 381, Book 3307 Page 93, and Book 6324 Page 2336.

item 16) Easement to St. Louis County Water Company, according to instrument recorded in Book 5808 Page 394.

Does not appear to affect this tract.

Easement for right of way for relocated state highway 340 (Clarkson Road) to state of Missouri, acting by and through the Missouri Highway and Transportation Commission, according to instrument recorded in Book 8625 Page 577.

This easement is shown as the right of way for this tract for Clarkson Road.

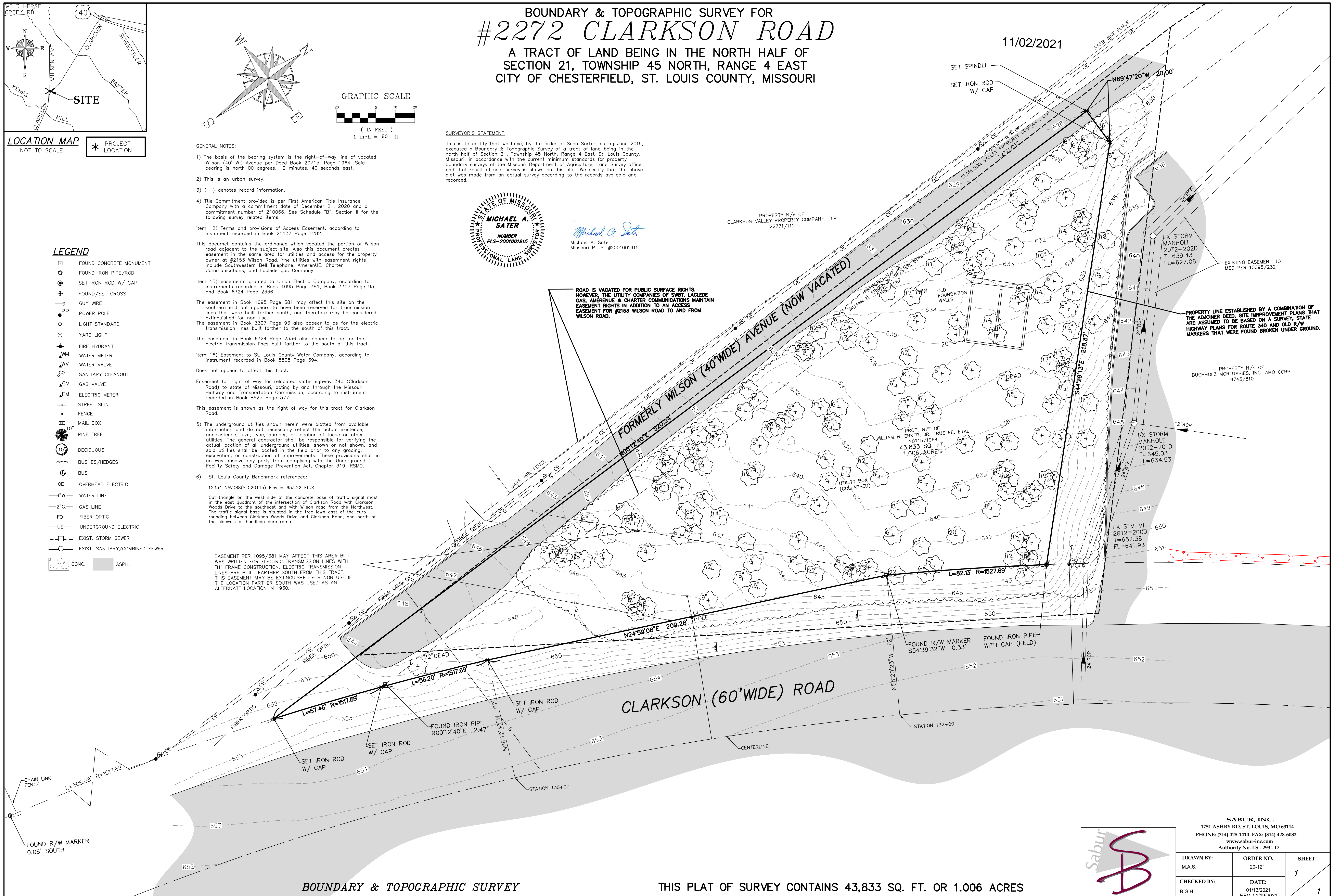
5) The underground utilities shown herein were plotted from available information and do not necessarily reflect the actual existence, nonexistence, size, type, number, or location of these or other utilities. The general contractor shall be responsible for verifying the actual location of all underground utilities, shown or not shown, and said utilities shall be located in the field prior to any grading, excavation, or construction of improvements. These provisions shall in no way absolve any party from complying with the Underground Facility Safety and Damage Prevention Act, Chapter 319, RSMO.

6) St. Louis County Benchmark referenced:
12334 NAVD88(SLC2011a) Elev = 653.22 FUS

Cut triangle on the west side of the concrete base of traffic signal mast in the east quadrant of the intersection of Clarkson Road with Clarkson Woods Drive to the southeast and with Wilson road from the Northwest. The traffic signal base is situated in the tree line east of the curb rounding between Clarkson Woods Drive and Clarkson Road, and north of the sidewalk of handicap curb ramp.

EASEMENT PER 1095/381 MAY AFFECT THIS AREA BUT WAS WRITTEN FOR ELECTRIC TRANSMISSION LINES WITH "H" FRAME CONSTRUCTION. ELECTRIC TRANSMISSION LINES ARE BUILT FARTHER SOUTH FROM THIS TRACT. THIS EASEMENT MAY BE EXTINGUISHED FOR NON USE IF THE LOCATION FARTHER SOUTH WAS USED AS AN ALTERNATE LOCATION IN 1930.

ROAD IS VACATED FOR PUBLIC SURFACE RIGHTS. HOWEVER, THE UTILITY COMPANIES OF SWBT, LACLEDE GAS, AMERENUE & CHARTER COMMUNICATIONS MAINTAIN EASEMENT RIGHTS IN ADDITION TO AN ACCESS EASEMENT FOR #2153 WILSON ROAD TO AND FROM WILSON ROAD.



PROPERTY LINE ESTABLISHED BY A COMBINATION OF THE ADJOINER DEED, SITE IMPROVEMENT PLANS THAT ARE ASSUMED TO BE BASED ON A SURVEY, STATE HIGHWAY PLANS FOR ROUTE 340 AND OLD R/W MARKERS THAT WERE FOUND BROKEN UNDER GROUND.

PROPERTY N/F OF
BUCHHOLZ MORTUARIES, INC. AMO CORP.
9743/810

BOUNDARY & TOPOGRAPHIC SURVEY

THIS PLAT OF SURVEY CONTAINS 43,833 SQ. FT. OR 1.006 ACRES



SABUR, INC.
1751 ASHBY RD. ST. LOUIS, MO 63114
PHONE: (314) 428-1414 FAX: (314) 428-6082
www.sabur-inc.com
Authority No. 15 - 293 - D

DRAWN BY: M.A.S.	ORDER NO. 20-121	SHEET 1
CHECKED BY: B.G.H.	DATE 01/13/2021 REV. 01/19/2021	

Trees worth noting due to their size but do not meet the criteria of a Monarch Tree. No Monarch Trees are present in this stand.

Tree #	Species	Common Name	DBH	Condition Rating	Monarch Tree	Notes
1	<i>Juglans nigra</i>	black walnut	24"	Poor	No	Due to condition, not worth preserving. Dead/decay parts
2	<i>Quercus imbricaria</i>	oak, shingle	25"	Poor	No	Moderate <i>Hypoxylon</i> canker - not worth preserving
3	<i>Celtis occidentalis</i>	common hackberry	21"	Fair	No	Codominant stems with included bark

Stand Observations/Comments:	Four plots were selected at random to measure stand basal area using a 10BAF prism. The average basal area of this stand is 90 and, according to a hardwood stocking chart, is considered "fully-stocked" at roughly 200 trees/acre. However, this data may be skewed due to the overwhelming presence of Amur honeysuckle (<i>Lonicera maackii</i>) which is densely distributed throughout the entire stand's understory.
	This approximate 1-acre stand is well-stocked with a diverse number of native hardwoods. The black walnut size class was 8-16", the red elm size class was 14-18", and hackberry was present in all generations of the stand. The SE portion of the stand had wetter soils where there were some American sycamore and sugar maple present. The eastern edge, along Clarkson Road, had the largest trees in the stand, none meeting the criteria of "Monarch Tree".
	Overstory Composition Black walnut, red elm, hackberry, and white ash. Red oak species along the edges.
	Midstory Composition Red elm, hackberry, black walnut, black cherry, persimmon, red maple, and sugar maple. Red oak species and sassafras along the eastern edge.
Understory Composition Limited regeneration: Hackberry and Amur honeysuckle	

SMS Group, LLC – Tree Stand Delineation

2272 Clarkson Road Chesterfield, MO 63017

Data Collected on March 24, 2021

Andy Berg, ISA Board Certified Master Arborist, IL-5361B