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# **Planning Commission Staff Report**

**Project Type:** Amended Architectural Elevations

Meeting Date: December 14, 2009

From: Charlie Campo, AICP Mara Perry, AICP

Project Planner Senior Planner

**Location:** AT&T Olive Subdivision, Lot 1 (Valvoline Instant Oil Change)

**Applicant:** Ashland Inc.

Description: <u>AT&T Olive Subdivision, Lot 1 (Valvoline Instant Oil Change)</u>:

Amended Architectural Elevations for a vehicle service center located on a 0.46 acre lot of land zoned "C-8" Planned Commercial District, located at the northwest corner of Olive Boulevard and

Woodchase Ln.

#### PROPOSAL SUMMARY

Ashland, Inc. has submitted Amended Architectural Elevations for Planning Commission review. The request is for amended architectural elevations to an existing, approximately 1,600 square foot vehicle service center. The exterior building material is brick with an internally lit barrel along the front of the building. The internally lit barrel will be removed and a decorative cornice will be added to the east and west sides of the front elevation of the building. A silver colored cornice detail will follow the top of the elevation on all sides of the building. Blue and red colored panels will be located on the rear of the building to serve as bay indicators. All materials will be pre-constructed and powder coated aluminum.

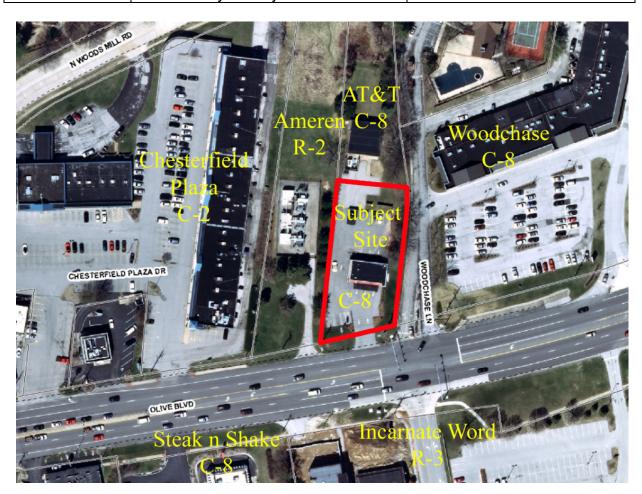
### LAND USE AND ZONING HISTORY OF SUBJECT SITE

On July 13, 1989, St. Louis County approved Ordinance 14,543, a request for a change of zoning from an "R2" Residence District 15,000 sq. ft. and a "C8" Planned Commercial District to form a new "C8" Planned Commercial District for a 1.602 acre tract of land. On October 5, 1989, St. Louis County amended Ordinance 14,543, to reduce the side yard setback along the west side of the lot from thirty-two (32) feet to twenty-seven (27) feet. St. Louis County further amended Ordinance 14,543, on March 15, 1990, to

require stormwater detention at the discretion of the Metropolitan St. Louis Sewer District. A Site Plan for the service station was approved on March 28, 1990 and then amended on August 24, 1990 to relocate the entrance to the site. On March 21, 1991, St. Louis County approved Ordinance 15,430, to change the legal description of the district and to increase the side yard setback along the east side of lot 2 from five (5) feet to ten (10) feet.

# Land Use and Zoning of Surrounding Properties:

Direction	Land Use	Zoning
North	Public Utility Facility	"C-8" Planned Commercial
South	Church	"R-3" Residence District
East	Retail	"C-8" Planned Commercial
West	Public Utility Facility	"R-2" Residence District



## **STAFF ANALYSIS**

#### Zoning

The subject site is currently zoned "C-8" Planned Commercial District under the terms and conditions of St. Louis County Ordinance 15,430. The Ordinance states that "Architectural elevations for all building facades shall be as approved by the Planning Commission on the Site Development Plan." The submittal was

reviewed against the requirements of St. Louis County Ordinance 15,430 and all applicable Zoning Ordinance Requirements. The site adheres to the applicable Zoning Ordinance requirements, the Architectural Review Board Guidelines and St. Louis County Ordinance 15,430.

### **DEPARTMENTAL INPUT**

Staff has reviewed the Amended Architectural Elevations and has found the application to be in conformance with the Comprehensive Plan, Zoning Ordinance and St. Louis County Ordinance 15,430.

#### **MOTION**

The following options are provided to the Planning Commission for consideration relative to this application:

- 1) "I move to approve (or deny) the Amended Architectural Elevations for AT&T Olive Subdivision, Lot 1 (Valvoline Instant Oil Change)."
- 2) "I move to approve the Amended Architectural Elevations for AT&T Olive Subdivision, Lot 1 (Valvoline Instant Oil Change) with the following conditions..." (Conditions may be added, eliminated, altered or modified)

Cc: City Administrator
City Attorney
Department of Planning and Public Works

Attachments: Amended Architectural Elevations
Photos of Existing Elevations
Site Development Plan





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