



IV. B.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

Architectural Review Board Staff Report

Project Type:	Site Development Section Plan
Meeting Date:	November 08, 2018
From:	Mike Knight, Planner
Location:	A 7.3 acre tract of land located northeast of the intersection of Wild Horse Creek Road and Old Chesterfield Road.
Description:	<u>Downtown Chesterfield Category C, Lot A (The Pearl at Wild Horse Creek)</u> SDSP: A Site Development Section Plan, Landscape Plan, Lighting Plan, Architectural Elevations and Architect's Statement of Design for a 7.3 acre tract of land located northeast of the intersection of Wild Horse Creek Road and Old Chesterfield Road.

PROPOSAL SUMMARY

This request is to allow for a development of a mixed-use building containing 173 residential units, 12,500 square-feet of community retail, and 10,500 square-feet of restaurant. The proposed facility is four to five stories in height, depending on elevation, and contains a parking garage positioned under the building. The building will be located in what is known as "Category C" of Downtown Chesterfield. The subject site is zoned "PC&R" Planned Commercial and Residence District and governed under the terms and conditions of City of Chesterfield Ordinances 2449 and 2990. The materials range from thin brick, EIFS, cultured stone, ribbed fiber cement, and ribbed metal panel.

HISTORY OF SUBJECT SITE

On March 19th, 2008 the City of Chesterfield City Council approved Ordinance 2449. This ordinance amended the previous governing zoning ordinance of the City of Chesterfield by changing the boundaries of 99 acres of the following: an "R-8" Residence District with a Planned Environmental Unit Procedure, "R-5" Residence District, "FPR-5" Residence District, "R-6A" PEU Residence District with a Planned Environmental Unit Procedure, "C-8" Planned Commercial District, "NU" Non-Urban District and "M-3" Planned Industrial District to **one 99 acre "PC & R" Planned Commercial & Residential District known as Downtown Chesterfield.**

City of Chesterfield Ordinance 2449 created one PC&R district that defined specific development criteria into three specific categories. The three categories were labeled in Ordinance 2449 as

Category A, Category B, and Category C. They were also defined by location, having specific category standards, and with a specific streetscape. **This Site Development Section Plan is 7.3 acres of the 22 acres that make up Category C, which is one of three categories that make up the 99 acres known as Downtown Chesterfield.** This section of land will continue to be referenced throughout the report.

On February 21st, 2018, the City of Chesterfield approved Ordinance 2990. This ordinance solely amended the legal description of the entire 99 acre PC&R district.

Currently there is a zoning petition for Downtown Chesterfield under review by City Council. This ordinance amendment has two main objectives. The first is to incorporate and re-zone a 0.4 acre Non-Urban “NU” parcel to the “PC&R” District. The second is to amend the development criteria of the governing ordinance in which separate Concept Plans would be submitted by defined Category vs. for the entire District. If approved, it essentially allows for 2 phases of development within Downtown Chesterfield, which directly impacts this project allowing it to proceed as submitted.



Figure 1: Aerial Image of Subject Site

STAFF ANALYSIS

The subject site is located north of Wild Horse Creek Road (WHCR), east of its intersection with Old Chesterfield Road (OCR) and west of the intersection with Chesterfield Parkway. The subject site is located directly south of Interstate 64. Given that WHCR and I-64 are both classified as major arterials according to the City’s functional classification system, and Chesterfield Parkway and OCR are classified as minor arterials, all facades will be highly visible to a large number of users. The area is designated Urban Core within the City of Chesterfield Comprehensive Land Use Plan and this development would be 7.3 acres of the 99 acre PC&R zoning district known as Downtown Chesterfield.

Figure 2 below is a visual representation to better understand the subject site in context with the 99 acres that make up Downtown Chesterfield in red. The figure also depicts the site’s surrounding Downtown Chesterfield and their current land use and zoning which is shown in Table 1 below.

The general area north of Downtown Chesterfield is immediately bordered by I-64. To the north of I-64 exists three office buildings currently occupied by the Reinsurance Group of America’s national headquarters and the Dierbergs Markets Corporate office.

The east is immediately bordered by Chesterfield Parkway West. To the east of Chesterfield Parkway West is undeveloped property.

The south is bordered by Burkhardt Place, which connects both the St. Louis County Library and the YMCA to Chesterfield Parkway West. To the west is primarily the residential subdivision, Reserve at Chesterfield Village, and undeveloped property. The area labeled as “G” currently has an active project known as Aventura at Wild Horse Creek, which consists of 2 buildings totaling 176 apartment units. Table 1 below further describes the surrounding area.



Figure 2: Downtown Chesterfield

Direction	Label	Current Land Use	Comprehensive Land Use Plan	Zoning District
North	A	Dierbergs	Urban Core	C8 - Planned Commercial
	B	RGA	Urban Core	C8 - Planned Commercial
East	C	Udeveloped	Urban Core	PC - Planned Commercial
South	D	Library	Urban Core	C8 - Planned Commercial
	E	YMCA	Urban Core / Multi-Family	PC - Planned Commercial
West	F	Reserve at Chesterfield	Multi-Family	R5 - Residence District
	G	Undeveloped	Urban Core/ Multi-Family	C8 - Planned Commercial
	H	Mobile Home Park	Urban Core	C7 - General Commercial

Table 1: Surrounding Locations

Zoning District:

The PC&R District, according to the Unified Development Code (UDC), is intended to provide development in the area of the City comprising a minimum of seventy (70) acres in size and located only in the area bounded on the east by State Route 340, on the west by Baxter Road, on the north by State Route 40/I-64, and on the south by Lydia Hill Drive/August Hill Drive. A PC&R District

development is intended to create a diverse residential and commercial mixed-use environment in which residential and commercial uses can be integrated pursuant to a downtown concept that encourages creative and coordinated design and architectural styles, efficient and effective pedestrian circulation, conservation of land resources, efficient and effective vehicular circulation, and where people can choose to live, work, eat, shop, enjoy cultural amenities and recreate. By definition, "downtown development" is mixed use, and usually follows one of two patterns (or an adaptation of both). First, as a vertical mix on a given parcel, land uses change from floor to floor within the same building. Typically, this pattern is residential above commercial (retail, professional services or office). The second pattern occurs when buildings or spaces of a single use are combined with those of other single uses. Examples are a street of residential buildings with commercial buildings occupying the corners or a commercial Main Street combined with residential side streets.

Land Use Designation:

The proposed development is located in the Urban Core within the City's Comprehensive Land Use Plan. Plan policies are the guiding principles for the Comprehensive Plan. These policies create the framework for decision-making related to future land use and development. Below are the plan policies that are relevant to this project given the location within the City's Urban Core.

Policy 1.8 Urban Core – The Urban Core should be developed to contain the highest density of mixed-use development in Chesterfield. It should serve as the physical and visual focus for the City and include both residential and commercial developments with parks, municipal services, and preservation of historic structures and areas, with cultural, entertainment and pedestrian amenities for its residents.

Policy 2.4 Higher Density Residential in Urban Core - New multiple-family residence should be located in or near the Urban Core.

Policy 3.6.2 Mixed-Use Development - The Urban Core should accommodate office, retail, high-density housing, government facilities, multi-modal transportation, cultural and entertainment facilities, and park space. Horizontal and vertical integration of uses is encouraged.

Policy 3.6.4 City Center - The development of a "City Center" within the Urban Core will create a community hub for public use, arts, with open-air restaurants, walkable shops, cultural and entertainment venues, and public gathering "places". Particular attention should be on providing first-floor pedestrian activities and architectural design.

Policy 3.6.7 Parking Structures - The use of parking structures in the Urban Core is encouraged.

Policy 7.2.10 Multi-modal Transportation Choices – Sites in the Urban Core should be designed for all types of transportation choices including pedestrian, bicycle, mass transit, and vehicular. Sites should be designed to provide for pedestrian, bicycle, mass transit, and vehicular inter-connectivity to adjacent sites.

General Requirements for Site Design:

This request is for one building containing residential, commercial, and restaurant uses on a highly visible 7.3 acre site. This will be the first development within the 99 acre Downtown Chesterfield.

A. Site Relationships

The UDC outlines specific desirable and undesirable practices within site relationships. The table below outlines desirable site practices within the code and how the SDSP relates to them.

Desirable Practices	Site Development Section Plan
Provide safe pedestrian movement between elements	A pedestrian walkway surrounds the entire building and extends to WHCR. It is unclear on the pedestrian movement to the future development to the east, west, and south.
Provide public plazas, courtyards, assembly areas, etc.	A plaza area is shown northwest of the roundabout.
Incorporate scenic views, fountains, public art, etc.	Public art in the form of a sculpture is located within the center of the roundabout.

Table 2: Desirable Practices

Below in Figure 3 is a color Site Development Section Plan for the proposed development. Although the applicant plans on future development shown to the east of the site, this review is solely for the western half of the roadway that enters the site from the south. Staff does point out that the architect’s statement of design submitted references a hotel on the future development to the east and a fountain feature that is not seen in this submittal. It is our understanding that the water feature has been removed and the hotel is still planned for future development but is not shown as it will be reviewed by the Board upon submittal of a separate Site Development Section Plan.

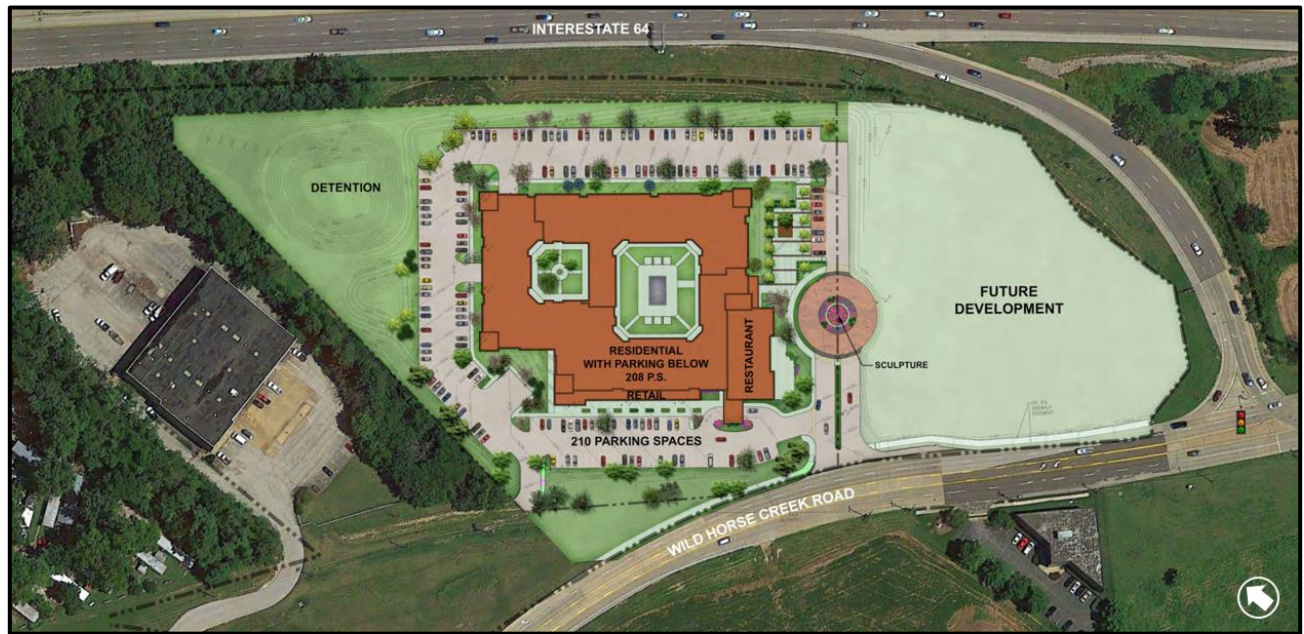


Figure 3: Color Site Plan

B. Circulation System and Access

The subject site will be served primarily by one entrance from WHCR, with a dedicated westbound right turn lane into the site. There is a planned private drive that is to be extended from the southwestern corner of the site to OCR in conjunction with this development. Vehicular traffic circulation can be seen around the entire building. A roundabout near the entrance of the site would provide access to the east that is labeled future development. Cross access easements will be required between the subject site and the future development to the east and west.

Pedestrian circulation can be seen completely circumnavigating the building, but it is unclear whether the primary pedestrian connection points to the developments to the east, west, and south. The City of Chesterfield Bikeable-Walkable plan designates a planned bike lane along this section of WHCR. The submittal currently does not show a bike lane along the major arterial.

Parking areas are distributed throughout the site around the building and through a parking garage under the building. The required parking for the site in accordance to the City's Unified Development Code is 485 spaces. The current site plan has 211 surface parking spaces and 208 garage parking spaces totaling 418 spaces, which does not meet the minimum parking standards for the site. Although it does not meet the requirements for the site, the governing ordinance dictates that required parking is calculated globally throughout Category C (22 acres) and not calculated on an individual lot basis.

C. Topography and Retaining Walls

The existing topography slopes from a southern high point down approximately 34 feet to the north on the buildable area of the site. There is approximately 31 feet of additional fall to the northern tip of the site. Modification to the grade is being proposed without the use of site retaining walls. The natural slope of the site is being utilized to facilitate the parking below the building. Other slope modifications provide for ADA accessibility throughout the improved area of the site.

General Requirements for Building Design:

This request is to allow for development of a mixed-use building. The mixture of uses consists of 173 multi-family residential units, 12,500 square feet of retail, and 10,500 square feet of restaurant. The building will be 4-5 stories in height depending on the elevation. The maximum height from top of roof to final elevation is 71'.

D. Scale, Design, Materials, and Color

The proposed building will be the first building specifically created for Downtown Chesterfield. The design standards for this building are elevated due to the proximity within the Urban Core, but also as it is the first phase in creating a Downtown Chesterfield. This building will begin to set the standard that would apply to the rest of the 99 acres that make up Downtown Chesterfield in which future developments will have to maintain cohesion and uniformity throughout the district.

The building design and finishes are coordinated and consistent on all building facades. The different components of the mixed-use buildings have different identities. The proposed restaurant space on the southeast corner features a glass enclosed corner stair leading up to a second-floor bar/dining

area with a rooftop patio space. The first floor commercial along WHCR projects from the building forming a continuous roof patio space utilized by the residents above. The individual residential living spaces are expressed by building facade offsets, changes in parapet height, balconies, along with material and color changes. The materials include thin brick, cultured stone, fiber cement siding, aluminum storefront, clear low-e-glass, architectural windows and doors. Brick cornice and other masonry details are used consistently on all sides of the building.

All of the color elevations are attached within the packet. Below in Figure 4 are two prominent elevations that can be seen from both WHCR and I-64. The “A” on top represents the view from WHCR detailing the mix of residential, retail, and restaurant and the “B” below represents the view from I-64 detailing all residential units and the entrance to the parking garage. Figure 5 is a rendered image of the site showing all three uses within the building.



Figure 4: Color Elevations



Figure 5: Rendering

E. Multi-Family Architecture

Section 04-01 includes specific requirements for multi-family architecture.

Provide an on-site pedestrian system with access to common ground areas – An on-site pedestrian system is highly visible around the building. Pedestrian access to all three neighboring sites lacks definition as to the designed access points for pedestrians to safely navigate.

Express architecturally the individual dwelling units within the building – In addition to other items mentioned in the report thus far, the proposal includes recessed and projecting balconies which help emphasize the individual units within the building.

Utilize color, material, and plane changes to articulate facades. Avoid monotonous or institutional designs – As discussed, the proposal includes color and material changes to avoid a monotonous design. Outdoor gathering areas are included on the first level of the development, which provide human scale and add additional detailing at the pedestrian level.

Respect the scale, proportion and character of the adjacent or predominant neighborhood – This development would be the first intentionally created for “Downtown Chesterfield” and would set the scale, proportion, and character for the next phase of development.

Provide functional recreation areas – An area in the form of a plaza can be seen northwest of the roundabout.

Provide outdoor space for each dwelling – In addition to common areas, balconies are provided. The balconies provide outdoor space for each unit and also add an element of depth to the proposed buildings.

Primary Building material - Primary building material shall be extended and installed so that no more than twelve (12) inches of concrete foundation wall is exposed.

F. Landscape Design and Screening

Landscaping is used to enhance the pedestrian experience, screen the service areas, and soften the building’s base where it meets the site. The applicant does not intend to preserve most of the existing canopy. A Tree Preservation Plan is required, but has not been submitted to the City. All site landscaping requirements will be required to be met including a 30’ landscape buffer along the major arterial I-64 and street trees along WHCR. The Landscape Plan is still under review within the Site Development Section Plan review process.

All mechanical units will be located on the roof and screened by the parapets. There will be three dumpsters located on the southwest portion of the site. The screening consists of brick veneer and the trash enclosure gates are to be metal deck painted to match the masonry wall.

G. Lighting

Lighting consists of utilitarian and decorative fixtures and is still under review within the Site Development Section Plan review process. There are three (3) fixtures in which two (2) are utilitarian and one (1) is decorative. Below in Figure 5 are examples of all three fixtures. All the lighting proposed is flat lensed and fully shielded.

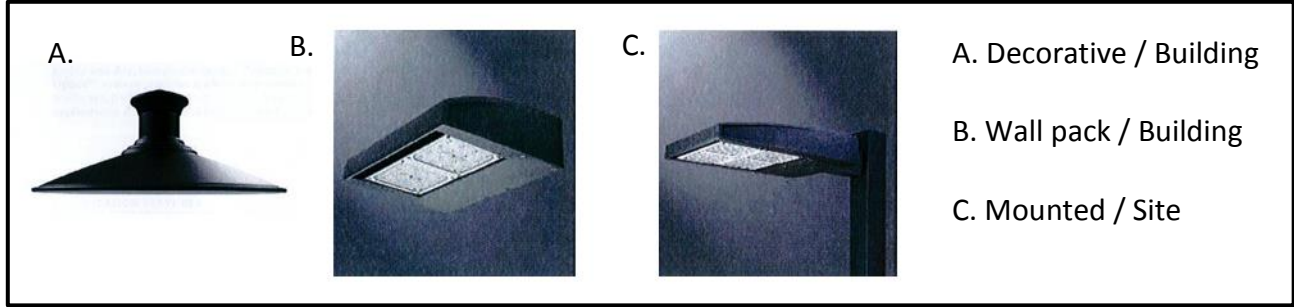


Figure 5: Lighting

GOVERNING ORDINANCE

As previously referenced, the governing ordinance for the 99 acres that comprise Downtown Chesterfield has three defined categories. The subject site is located in what is known as Category C. This is the 22 acres north of WHCR and can be seen in Figure 6.

Category C has specific standards outlined in the governing ordinance that reference building placement, building volume, floor heights, façade elements, outdoor space, and building function. The ordinance also provides a specific exhibit that details a streetscape to which the development should adhere.

Section I.O of the governing ordinance states that upon application from a petitioner, any performance standard provided in the Attachment "A" or required by any other district regulation or Ordinance of the City may be supplemented, modified, or altered provided such supplement, modification, or alteration will further the purpose and intent of the PC&R District. A public hearing is not required in the process and recommendation by Planning Commission will be forwarded to City Council for final approval. On the following page is four modifications that the applicant is seeking and Figure 7 which details the required streetscape outlined within the ordinance.



Figure 6: Category Location

The applicant is seeking four modifications from the category standards set forth in the governing ordinance.

1. Current plan states a 30' landscape buffer along WHCR; Exhibit 1 has a maximum of 10'.
2. A 12' sidewalk easement is required in Exhibit A and not shown on the plan
3. The maximum distance a building can be from the WHCR right of way is 52' according to Exhibit A. The minimum distance of a building from WHCR is no less than 100' represented on the current submittal.
4. Exhibit A specifically removes parking in between WHCR and the "build to" line unlike Categories A & B. This submittal has 2 rows of parking between WHCR and the development on Lots A and B.

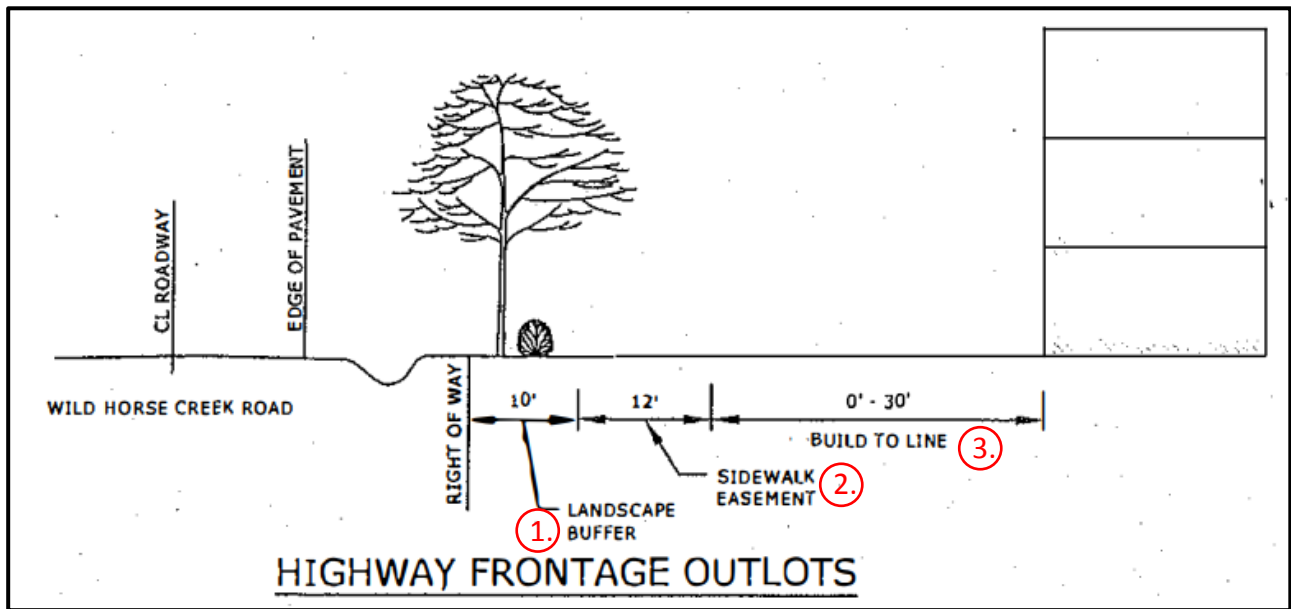


Figure 7: Exhibit 1 from Governing Ordinance

The applicant has provided a modification request stating the intent of the request is to achieve the goals and objectives of the PC&R district. This site focuses on the center of the proposed development with a plaza, roundabout, public art, sidewalks and park benches. The proposed buildings will surround the plaza and bring a "Downtown" feel to the area and further the intent of the PC&R. The applicant will essentially be building the streetscape off of the internal drive vs. WHCR.

Staff acknowledges and is supportive of the center drive aisle dictating an urban "Downtown" streetscape vs. the major arterial of WHCR. Staff encourages more interaction with the street and the building's urban form as dictated in the streetscape identified in Exhibit 1 of the governing

ordinance. As proposed the parking along the center drive aisle and the expansive green areas in between the aisle and building deviates from the downtown essence that exists within the streetscape.

The Architectural Review Board's recommendation on the requested modifications will be provided to the Planning Commission. Attached to this packet is the specific Category C Standards for Downtown Chesterfield, the defined streetscape within the governing ordinance known as Exhibit 1, and the modification request that has been submitted by the applicant.

DEPARTMENT INPUT

Be advised, this project is still going through development review by City Staff and will not proceed to the Planning Commission until all outstanding items have been addressed. All recommendations made by the ARB will be included in Staff's report to the Planning Commission.

Staff requests review and recommendation on this submittal for Downtown Chesterfield Category C, Lot A (The Pearl at Wild Horse Creek) Site Development Section Plan.

MOTION

The following options are provided to the Architectural Review Board for consideration relative to this application:

- 1) "I move to forward the Site Development Section Plan, Landscape Plan, Lighting Plan, Architectural Elevations and Architect's Statement of Design for Downtown Chesterfield Category C, Lot A (The Pearl at Wild Horse Creek) SDSP, as presented, with a recommendation for approval (or denial) to the Planning Commission."

- 2) "I move to forward the Site Development Section Plan, Landscape Plan, Lighting Plan, Architectural Elevations and Architect's Statement of Design for Downtown Chesterfield Category C, Lot A (The Pearl at Wild Horse Creek) SDSP, to the Planning Commission with the following recommendations..."

Attachments

1. Architectural Review Packet Submittal
2. Category C Standards
3. Exhibit 1
4. Modification Request



**ARCHITECTURAL REVIEW BOARD
Project Statistics and Checklist**

Date of First Comment Letter Received from the City of Chesterfield 09/26/2018

Project Title: The Pearl at Wild Horse Creek **Location:** 16511 Wild Horse Creek Road

Developer: Chesterfield Village Inc. & Brinkmann Holdings LLC (Owner Under Contract) **Architect:** TR,i Architects **Engineer:** Stock & Associates

PROJECT STATISTICS:

Size of site (in acres): 7.3 acres **Total Square Footage:** 292,000 S.F **Building Height:** 71' Maximum

Proposed Usage: Mixed-use: Residential, Commercial and Restaurant

Exterior Building Materials: Brick, cultured stone, ribbed metal panel, fiber cement siding, storefront & glass

Roof Material & Design: T.P.O. - Low slope (1/4" per foot)

Screening Material & Design: Parapet walls & metal panel

Description of art or architecturally significant features (if any): Re: Attached project narrative

ADDITIONAL PROJECT INFORMATION:

Checklist: Items to be provided in an 11" x 17" format

- Color Site Plan with contours, site location map, and identification of adjacent uses.
- Color elevations for all building faces.
- Color rendering or model reflecting proposed topography.
- Photos reflecting all views of adjacent uses and sites.
- Details of screening, retaining walls, etc.
- Section plans highlighting any building off-sets, etc. (as applicable)
- Architect's Statement of Design which clearly identifies how each section in the Standards has been addressed and the intent of the project.
- Landscape Plan.
- Lighting cut sheets for any proposed building lighting fixtures. (as applicable)
- Large exterior material samples. (to be brought to the ARB meeting)
- Any other exhibits which would aid understanding of the design proposal. (as applicable)
- Pdf files of each document required.



October 26, 2018

Joseph M. Knight
City Planner
City of Chesterfield

**Re: The Pearl at Wild Horse Creek
TR,i Project #: 18-030
ARCHITECT'S STATEMENT OF DESIGN**

To Whom it may concern,

As required by Chesterfield's Unified Development Code the following is our Architect's Statement of Design responding to the Architectural review design standards as they apply to our project.

General requirements for site design:

1. Site relationships: The proposed development is located between Interstate 64 and Wild Horse Creek Road adjacent to the East bound I-64 off ramp. The only parcel with direct connection to the site is an existing warehouse development to the west. There are two vehicular access points to the site. The primary vehicular access is via full service curb cut on Wild Horse Creek Road. A secondary access is provide extending west through the existing warehouse site to connect to this development to Old Chesterfield Road. A future hotel use is depicted on the east side of the site plan to demonstrate the over-all site development concept. The proposed and future buildings are organized on the site to define an outdoor space. A water feature will anchor the center of the outdoor space along with a roundabout, which will serve to calm vehicular speed and provide for distribution of traffic within the development. In the center of the proposed development would be a plaza, comprised of a round-a-bout, public art, sidewalks, and park benches. This would be the focal point as one enters the development. All vehicles entering the development would navigate through the round-a-bout, similar to Carondelet Plaza in Clayton, Missouri. The proposed buildings will surround the plaza and bring a "downtown" feel to the area and further the intent of the PC&R by promoting efficient and effective pedestrian and vehicle circulation. Parking for the development will be both surface and below grade garage parking. Residents would utilize the garage while the surface parking would be used by patrons of the restaurant and boutique retail. A valet service will also be a component of the restaurant.
2. Circulation system and access: The site organization emphasizes pedestrian connectivity of uses. Pedestrian access to the site is provide via a sidewalk running the length of the property

along Wild Horse Creek Road. The development plan includes separation between vehicular circulation and pedestrian circulation with a network of public sidewalks within the development. The comprehensive pedestrian circulation system connects street to building, parking areas to building and building use to building use. Parking areas are distributed throughout the site to minimize impact, with forty eight percent (48%) of the proposed initial phase parking being located below the building, away from public view. Landscaping is used along both the internal and external network of sidewalks to add interest to the pedestrian experience. The design incorporates extensive landscaping along the interstate and Wild Horse Creek Road, which can be extended to adjacent properties as they are developed/re-developed, thus providing site connectivity and continuity.

3. Topography: The existing topography slopes from a southern high point down approximately 34 feet to the north on the buildable area of the site. There is approximately 31 feet of additional fall to the northern tip of the site. Modification to the grade is being proposed without the use of site retaining walls. The natural slope of the site is being utilized to facilitate parking below the building. Other slope modifications provide for handicapped pedestrian accessibility throughout the improved areas of the site. Extensive landscaping is used for screening, buffering, and enhancement to the site.
4. Retaining walls: No retaining walls are being proposed.

General requirements for building design:

1. Scale: There are currently no adjacent projects that set a precedent for development consistent with the City of Chesterfield design standards. The site is located between Interstate 64 and Wild Horse Creek Road. This creates the need for the project to express a scale appropriate to fit the smaller scale, more pedestrian oriented feel needed along the Wild Horse Creek Road frontage and the larger highway scaled projects along the I-64 frontage. The façade facing Wild Horse Creek Road utilizes a variety of building form, height and materials to create a dynamic and engaging expression to the public. The I-64 frontage maintains a similar level of animation and use of quality building materials making that facade commensurate with neighboring highway developments.
2. Design: The building design and finishes are coordinated on all sides of the building. The project has a unique character without influence of any corporate identity. The different components of this mixed-use building have individual identities. The proposed restaurant space on the southeast corner features a glass enclosed corner stair leading up and a second-floor bar/dining area with rooftop patio space. The first floor commercial along Wild Horse Creek Road projects from the building forming a continuous roof patio space utilized by the residents above. Decorative lighting, glass storefront, cultured stone and brick are used to create a pedestrian friendly facade. The individual residential living spaces are expressed by building façade offsets, changes in parapet height, balconies, material and color changes.
3. Materials and Colors: Warm earth tone finishes are used throughout the exterior design that are neutral and compatible with many other pallets. The materials include brick, cultured stone, cementitious siding, aluminum storefront, clear low-e glass, architectural windows and doors. Brick cornice and other masonry details are used consistently on all sides of the building.

4. Landscape design and screening: Landscaping is used along the street to create connectivity to adjacent sites. Within the site landscaping is used to enhance the pedestrian experience, screen the service areas, and soften the building's base where it meets the site.
5. Signage: It is understood that signage is reviewed separately.
6. Lighting: All exterior lighting will be fully cut off and will adhere to Chesterfields UDC. Cut sheets of the lighting are included for review, they complement the buildings style.

I trust this information meets your needs. Please advise if you have any questions or comments.

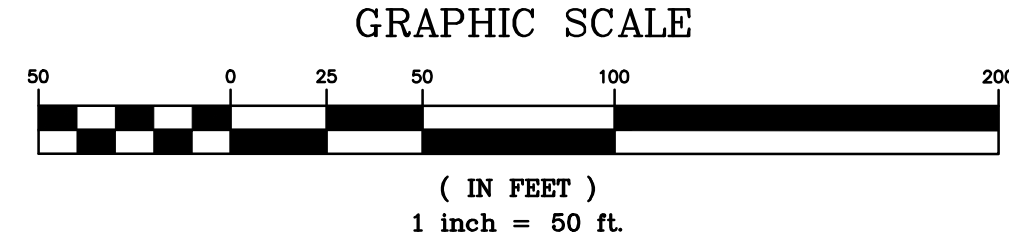
Respectfully,

A handwritten signature in black ink, appearing to read "Daniel Tate". The signature is written in a cursive, flowing style with a large initial "D".

Daniel Tate
Senior Design Architect

SHEET INDEX

- SDSP-1 - SITE DEVELOPMENT SECTION PLAN
SDSP-2 - PHOTOMETRIC PLAN
L-1 - LANDSCAPE PLAN
PSP-1 - ARCHITECTURE ELEVATIONS



SITE DEVELOPMENT SECTION PLAN

PART OF U.S. SURVEYS 123 AND 2031 TOWNSHIP 45 NORTH, RANGE 4 EAST
CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI

ZONED: PLANNED COMMERCIAL + RESIDENCE DISTRICT "PC+R" PER ORD. NO'S #2449 + #2990



STOCK & ASSOCIATES Consulting Engineers, Inc.
2517 Chesterfield Business Parkway
St. Louis, MO 63005 PH: (636) 590-9000
5901-9000 FAX: (636) 590-9000
e-mail: general@stockass.com
Web: www.stockass.com

PERTINENT DATA

LOT A
OWNER: CHESTERFIELD VILLAGE INC
OWNER UNDER CONTRACT: BRINKMANN HOLDINGS LLC
LOT A AREA: 7.306 Acres ±
EXISTING ZONING: "PC&R" PLANNED COMMERCIAL & RESIDENCE (ORD. #2449 & #2990)
SITE ADDRESS: 16461, 16491, 16493, 16495, 16503, 16511 WILD HORSE CREEK CHESTERFIELD, MISSOURI 63005

ST. LOUIS COUNTY STANDARD NOTES

- 1. ALL PROPOSED IMPROVEMENTS SHALL BE CONSTRUCTED TO ST. LOUIS COUNTY STANDARDS.
2. NO SLOPES WITHIN ST. LOUIS COUNTY RIGHT-OF-WAY SHALL EXCEED 3% (HORIZONTAL) TO 1% (VERTICAL).
3. STORM WATER SHALL BE DISCHARGED AT AN ADEQUATE NATURAL DISCHARGE POINT. SINKHOLES ARE NOT ADEQUATE DISCHARGE POINTS.
4. ALL PROPOSED ACCESS TO ST. LOUIS COUNTY ROADS SHALL MEET MINIMUM ST. LOUIS COUNTY SIGHT DISTANCE REQUIREMENTS.
5. ALL SIDEWALKS AND ASSOCIATED ACCESSIBILITY IMPROVEMENTS WITHIN RIGHT-OF-WAY SHALL BE CONSTRUCTED TO ST. LOUIS COUNTY ADA STANDARDS.
6. A SIGNED/SEALED NOTE SHALL BE ADDED TO THE CONSTRUCTION PLANS INDICATING THAT THE UNIMPROVED EXISTING SIDEWALK/PEDESTRIAN PATH ALONG THE PROJECT FRONTAGE MEETS CURRENT ST. LOUIS COUNTY ADA STANDARDS.
7. ALL GRADING AND DRAINAGE SHALL BE IN CONFORMANCE WITH ST. LOUIS COUNTY AND MSD STANDARDS.
8. ALL HYDRANTS, POWER POLES OR OTHER POTENTIAL OBSTRUCTIONS WITHIN THE ST. LOUIS COUNTY ROAD RIGHT-OF-WAY SHALL HAVE A MINIMUM TWO (2) FOOT SETBACK FROM FACE OF CURB OR EDGE OF PAVEMENT, AS DIRECTED BY THE ST. LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC.
9. ANY ENTITY THAT PERFORMS WORK ON ST. LOUIS COUNTY MAINTAINED PROPERTY SHALL PROVIDE THE COUNTY WITH A CERTIFICATE OF INSURANCE EVIDENCING GENERAL LIABILITY COVERAGE (BODILY INJURY AND PROPERTY DAMAGE) IN THE AMOUNTS SPECIFIED AS THE LIMITS OF LIABILITY SET BY THE STATE FOR PUBLIC ENTITIES. SUCH CERTIFICATE SHALL INCLUDE ST. LOUIS COUNTY AS AN ADDITIONAL INSURED AND SHALL BE PROVIDED PRIOR TO THE ISSUANCE OF ANY PERMIT. CERTIFICATE SHALL PROVIDE FOR A 30 DAY POLICY CANCELLATION NOTICE TO ST. LOUIS COUNTY. UPON REQUEST, THE COUNTY WILL PROVIDE THE SPECIFIC AMOUNTS FOR BOTH PER PERSON AND PER OCCURRENCE LIMITS.
10. PRIOR TO "SPECIAL USE PERMIT" ISSUANCE BY THE ST. LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC, A SPECIAL CASH ESCROW OR A SPECIAL ESCROW SUPPORTED BY AN IRREVOCABLE LETTER OF CREDIT, MAY BE REQUIRED TO BE ESTABLISHED WITH THE ST. LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC TO GUARANTEE COMPLETION OF THE REQUIRED ROADWAY IMPROVEMENTS.
11. CONTINUOUS PEDESTRIAN ACCESS SHALL BE PROVIDED DURING THE CONSTRUCTION PROCESS. PRIOR TO THE START OF CONSTRUCTION, ADEQUATE PEDESTRIAN ACCESS AROUND THE SITE SHALL BE PROVIDED AND VERIFIED. NO EXISTING SIDEWALK SHALL BE REMOVED WITHOUT PROVIDING ADEQUATE PEDESTRIAN FACILITIES AND ROUTES DURING CONSTRUCTION ACTIVITIES.

GENERAL NOTES

- 1. BOUNDARY AND TOPOGRAPHIC SURVEY BY STOCK & ASSOCIATES CONSULTING ENGINEERS, INC.
2. ALL UTILITIES SHOWN HAVE BEEN LOCATED BY THE ENGINEER FROM AVAILABLE RECORDS. THEIR LOCATION SHOULD BE CONSIDERED APPROXIMATE. THE CONTRACTOR HAS THE RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES, PRIOR TO CONSTRUCTION, TO HAVE EXISTING UTILITIES FIELD LOCATED.
3. NO GRADE SHALL EXCEED 3:1 SLOPE.
4. GRADING AND STORM WATER PER M.S.D., MODOT, ST. LOUIS COUNTY, THE CITY OF CHESTERFIELD AND THE MONARCH CHESTERFIELD LEVEE DISTRICT.
5. STORMWATER SHALL BE DISCHARGED AT ADEQUATE NATURAL DISCHARGE POINT. SINKHOLES ARE NOT ADEQUATE DISCHARGE POINTS.
6. ALL UTILITIES WILL BE INSTALLED UNDERGROUND.
7. SITE DEVELOPMENT SHALL BE IN ACCORDANCE WITH RECOMMENDATIONS AS OUTLINED IN THE GEOTECHNICAL EXPLORATION TITLED AND ALL IRS SUPPLEMENTAL PROVISIONS AND ADDENDUMS.
8. SIGNAGE APPROVAL IS A SEPARATE PROCESS

NOTE: PROJECT SHALL COORDINATE WITH COUNTY PROJECT AR-1756



LOT A
Beginning at a found iron pipe, located on the north right-of-way line of Olive Street Road, variable width, said point also being the southeast corner of a tract of land as conveyed to Chesterfield Village, Inc. by instrument recorded in Book 16957, Page 288 of the St. Louis County records; thence along the eastern line of said Chesterfield Village, Inc. tract, North 06 degrees 35 minutes 49 seconds East, 601.75 feet to a found iron rod; thence North 47 degrees 51 minutes 43 seconds East, 36.14 feet to the southwest right-of-way line of Interstate Route 64 (US 40-61), variable width; thence along said right-of-way line the following: South 42 degrees 08 minutes 06 seconds East, 197.77 feet to a point of curvature to the right having a radius of 11,157.00 feet; thence along said curve with an arc length of 630.55 feet and a chord which bears South 40 degrees 29 minutes 29 seconds East, 639.46 feet thence departing said right-of-way line, South 50 degrees 04 minutes 02 seconds West, 459.43 feet to the point of non-tangential curve to the left having a radius of 991.00 feet; thence along said curve with an arc length of 121.42 feet and a chord which bears North 56 degrees 15 minutes 21 seconds West, 121.42 feet; thence North 44 degrees 16 minutes 49 seconds West, 279.36 feet to the direct southwestern prolongation of the east line of above said Chesterfield Village, Inc. tract; thence along said prolongation line, North 06 degrees 35 minutes 49 seconds East, 38.67 feet to the POINT OF BEGINNING. Containing 318,221 square feet or 7.305 acres, more or less.

LOT B
Commencing at a found iron pipe, located on the north right-of-way line of Olive Street Road, variable width, said point also being the southeast corner of a tract of land as conveyed to Chesterfield Village, Inc. by instrument recorded in Book 16957, Page 288 of the St. Louis County records; thence departing said right-of-way line and along the northern and southwestern lines of proposed A the following courses and distances: South 06 degrees 35 minutes 49 seconds West, 38.67 feet; South 44 degrees 16 minutes 49 seconds West, 279.36 feet to the beginning of curve to the right having a radius of 991.00 feet and along said curve with an arc length of 121.42 feet and a chord which bears South 56 degrees 15 minutes 21 seconds East, 121.42 feet to the POINT OF BEGINNING of the herein described tract; thence North 50 degrees 04 minutes 02 seconds East, 459.43 feet to the southwestern right-of-way line of Interstate Route 64 (US 40-61), variable width, said point also being located on a non-tangential curve to the right having a radius of 11,157.00 feet; thence along said right-of-way line the following courses and distances: along left said curve with an arc length of 69.78 feet and a chord which bears South 38 degrees 40 minutes 17 seconds East, 69.78 feet; North 77 degrees 04 minutes 54 seconds East, 0.27 feet to the point of non-tangential curve to the right having a radius of 5,664.58 feet; thence along said curve with an arc length of 38.40 feet and a chord which bears South 34 degrees 46 minutes 22 seconds East, 38.40 feet; South 24 degrees 30 minutes 03 seconds East, 125.81 feet; South 14 degrees 23 minutes 51 seconds West, 134.14 feet; South 03 degrees 27 minutes 11 seconds East, 145.49 feet; South 40 degrees 13 minutes 55 seconds West, 105.00 feet; South 07 degrees 43 minutes 17 seconds West, 81.39 feet and South 40 degrees 13 minutes 54 seconds West, 7.58 feet to a found iron rod located on the northeastern right-of-way line of Wild Horse Creek Road, variable width; thence along said right-of-way line the following courses and distances: North 49 degrees 03 minutes 33 seconds West, 44.62 feet to a found iron pipe located at the point of curvature to the right having a radius of 1,109.92 feet; along said curve with an arc length of 108.06 feet and a chord which bears North 47 degrees 03 minutes 42 seconds West, 108.02 feet; North 44 degrees 16 minutes 21 seconds West, 98.51 feet to a point of curvature to the left having a radius of 991.00 feet and along said curve with an arc length of 109.10 feet and a chord which bears North 47 degrees 25 minutes 35 seconds West, 109.05 feet; thence departing said right-of-way line and continuing along a last said curve to the left having a radius of 991.00 feet and an arc length of 37.42 feet and a chord which bears North 51 degrees 39 minutes 43 seconds West, 37.42 feet to the POINT OF BEGINNING. Containing 151,527 square feet or 3.474 acres, more or less.

OPEN SPACE:

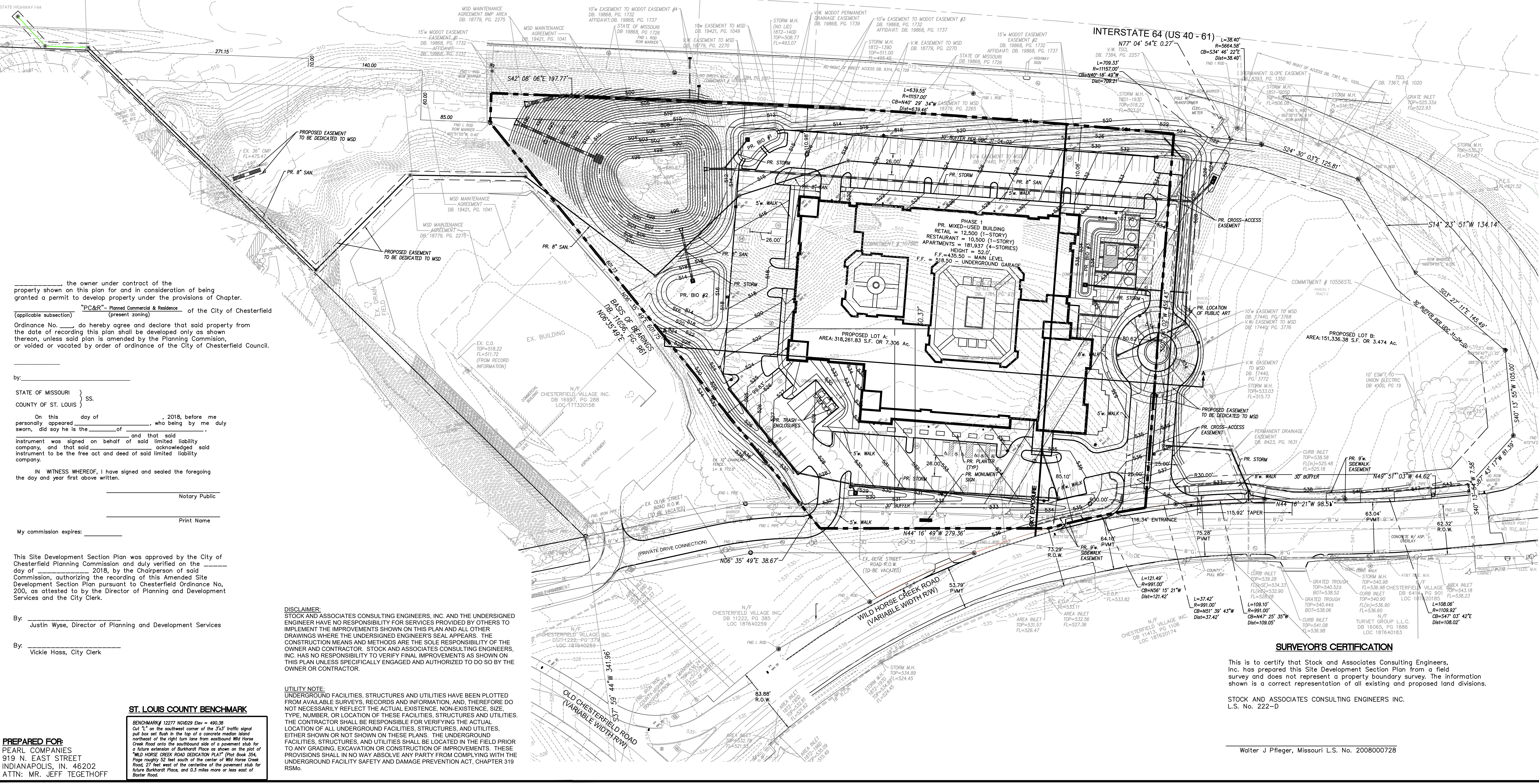
PHASE 1 (LOT A) PROVIDED: 44.48% (141,571 S.F./318,261 S.F.)
FUTURE PHASE 2 (LOT B) PROVIDED: 36.61% (55,406 S.F./151,336 S.F.)

PARKING:

Table with columns for Required Spaces, Provided Surface Spaces, Total Provided Spaces, Total Required Lots A, and Total Provided Lots A. Includes details for Retail, Restaurant, and Residential uses.

BUILDING HEIGHT REGULATIONS

MINIMUM BUILDING HEIGHT ALL CATEGORIES = 25 FEET
MAXIMUM BUILDING HEIGHT ALL CATEGORIES = 150 FEET
FIRST STORY HEIGHT ALL CATEGORIES = 12-25 FEET
UPPER STORY HEIGHT ALL CATEGORIES = 8-18 FEET



I, the owner under contract of the property shown on this plan for and in consideration of being granted a permit to develop property under the provisions of Chapter. "PC&R"-Planned Commercial & Residence of the City of Chesterfield (applicable subsection) do hereby agree and declare that said property from the date of recording this plan shall be developed only as shown thereon, unless said plan is amended by the Planning Commission, or voided or vacated by order of ordinance of the City of Chesterfield Council.

STATE OF MISSOURI
COUNTY OF ST. LOUIS
On this day of 2018, before me personally appeared, who being by me duly sworn, did say he is the and that said instrument was signed on behalf of said limited liability company, and that said acknowledged said instrument to be the free act and deed of said limited liability company.
IN WITNESS WHEREOF, I have signed and sealed the foregoing the day and year first above written.

This Site Development Section Plan was approved by the City of Chesterfield Planning Commission and duly verified on the day of 2018, by the Chairperson of said Commission, authorizing the recording of this Amended Site Development Section Plan pursuant to Chesterfield Ordinance No. 200, as attested to by the Director of Planning and Development Services and the City Clerk.

DISCLAIMER: STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC. AND THE UNDERSIGNED ENGINEER HAVE NO RESPONSIBILITY FOR SERVICES PROVIDED BY OTHERS TO IMPLEMENT THE IMPROVEMENTS SHOWN ON THIS PLAN AND ALL OTHER DRAWINGS WHERE THE UNDERSIGNED ENGINEER'S SEAL APPEARS. THE CONSTRUCTION MEANS AND METHODS ARE THE SOLE RESPONSIBILITY OF THE OWNER AND CONTRACTOR. STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC. HAS NO RESPONSIBILITY TO VERIFY FINAL IMPROVEMENTS AS SHOWN ON THIS PLAN UNLESS SPECIFICALLY ENGAGED AND AUTHORIZED TO DO SO BY THE OWNER OR CONTRACTOR.

UTILITY NOTE: UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS, RECORDS AND INFORMATION, AND THEREFORE DO NOT NECESSARILY REFLECT THE ACTUAL EXISTENCE, NON-EXISTENCE, SIZE, TYPE, NUMBER, OR LOCATION OF THESE FACILITIES, STRUCTURES AND UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE ACTUAL LOCATION OF ALL UNDERGROUND FACILITIES, STRUCTURES, AND UTILITIES, EITHER SHOWN OR NOT SHOWN ON THESE PLANS. THE UNDERGROUND FACILITIES, STRUCTURES, AND UTILITIES SHALL BE LOCATED IN THE FIELD PRIOR TO ANY GRADING, EXCAVATION OR CONSTRUCTION OF IMPROVEMENTS. THESE PROVISIONS SHALL IN NO WAY ABSOLVE ANY PARTY FROM COMPLYING WITH THE UNDERGROUND FACILITY SAFETY AND DAMAGE PREVENTION ACT, CHAPTER 319 RSMo.

ST. LOUIS COUNTY BENCHMARK
BENCHMARK # 1227 NG029 Elev = 490.38
Cut 1" on the southwest corner of the 24" traffic signal pull box set flush in the top of a concrete median island northeast of the right turn lane from westbound Wild Horse Creek Road onto the northbound side of a pavement stub for a future extension of Burkhardt Place as shown on the plot of Wild Horse Creek Road Dedication Plat (Plat Book 254, Page roughly 52 feet south of the center of Wild Horse Creek Road, 27 feet from the centerline of the pavement stub for future Burkhardt Place, and 0.3 miles more or less east of Barker Road.

PREPARED FOR: PEARL COMPANIES
919 N. EAST STREET
INDIANAPOLIS, IN 46202
ATTN: MR. JEFF TEGETHOFF

SURVEYOR'S CERTIFICATION
This is to certify that Stock and Associates Consulting Engineers, Inc. has prepared this Site Development Section Plan from a field survey and does not represent a property boundary survey. The information shown is a correct representation of all existing and proposed land divisions.

Table with columns for Drawn By, Checked By, Date, Job No., Base Map, S.L.C. Map #, M.D.N.R. #, Sheet Title, and Sheet No. Includes signature of Walter J. Pfeiffer.

WILDHORSE
SITE DEVELOPMENT SECTION PLAN FOR:
16461, 16491, 16493, 16495, 16503, 165011 WILD HORSE CREEK ROAD
CHESTERFIELD, MO 63005 & 63017

Table with columns for Drawn By, Checked By, Date, Job No., Base Map, S.L.C. Map #, M.D.N.R. #, Sheet Title, and Sheet No. Includes signature of George M. Stock, P.E.

LANDSCAPE REQUIREMENTS:

STREET TREES: 1 per 50 LF FRONTAGE (or part thereof)

- 400.78 LF FRONTAGE @ WILDHORSE CREEK RD. (Lot A), REQUIRING NINE (9) TREES; NINE (9) TREES PROVIDED
- 290.92 LF FRONTAGE @ WILDHORSE CREEK RD. (Lot B), REQUIRING SIX (6) TREES; SIX (6) TREES PROVIDED
- 337.30 LF FRONTAGE @ WILDHORSE CREEK RD. (Lot C), REQUIRING SEVEN (7) TREES; SEVEN (7) TREES PROVIDED

NOTE: NO PARKING SPACE SHALL BE FURTHER THAN FIFTY (50) FEET FROM A TREE.

TREE GROUPINGS:

TYPE	QTY.	PERCENTAGE
DECIDUOUS:	13	37%
ORNAMENTAL:	12	34%
EVERGREEN:	10	29%

• A MINIMUM OF 20% OF TREES SHALL COME FROM THREE CATEGORIES: DECIDUOUS / ORNAMENTAL / EVERGREEN

• 13 FAST GROWTH (37%) AND 22 SLOW/MEDIUM GROWTH (63%)

• ALL STREET TREES SHALL BE MIN. 2.5" CALIPER WITH A MAXIMUM OF 20% OF TREES OF ONE SPECIES THROUGHOUT

• TOTAL SITE: 318,261 SF @ LOT A and 151,336 SF @ LOT B

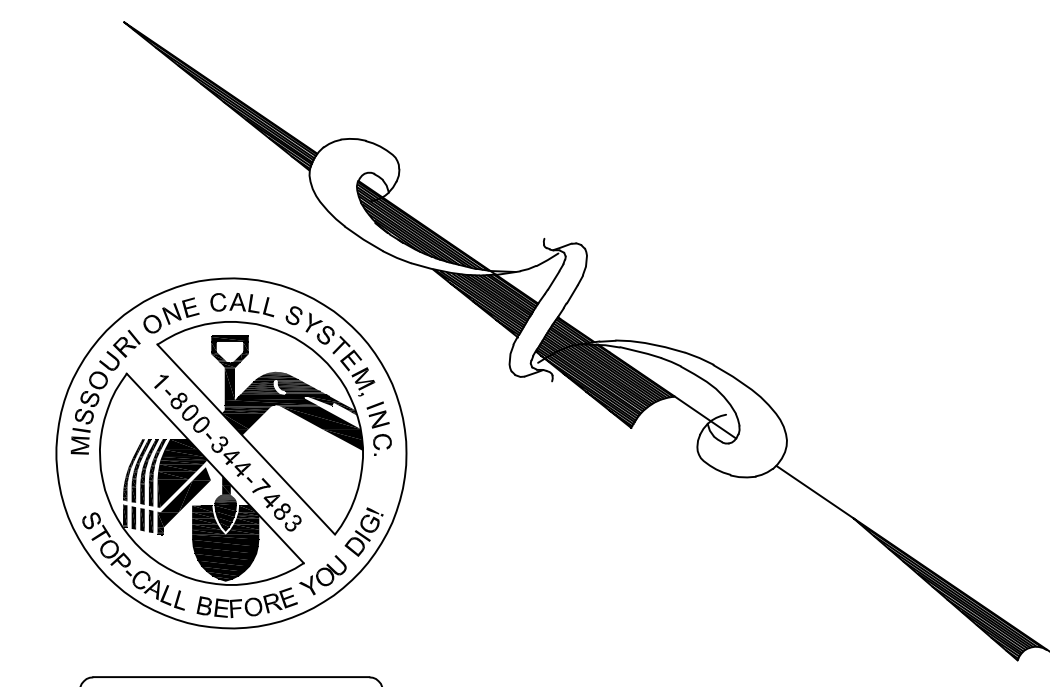
OPEN SPACE: 141,571 SF or 44.48% @ LOT A
55,406 SF or 36.61% @ LOT B

• BUILDING AREAS: 85,580 SF @ LOT A and 19,176 SF @ LOT B

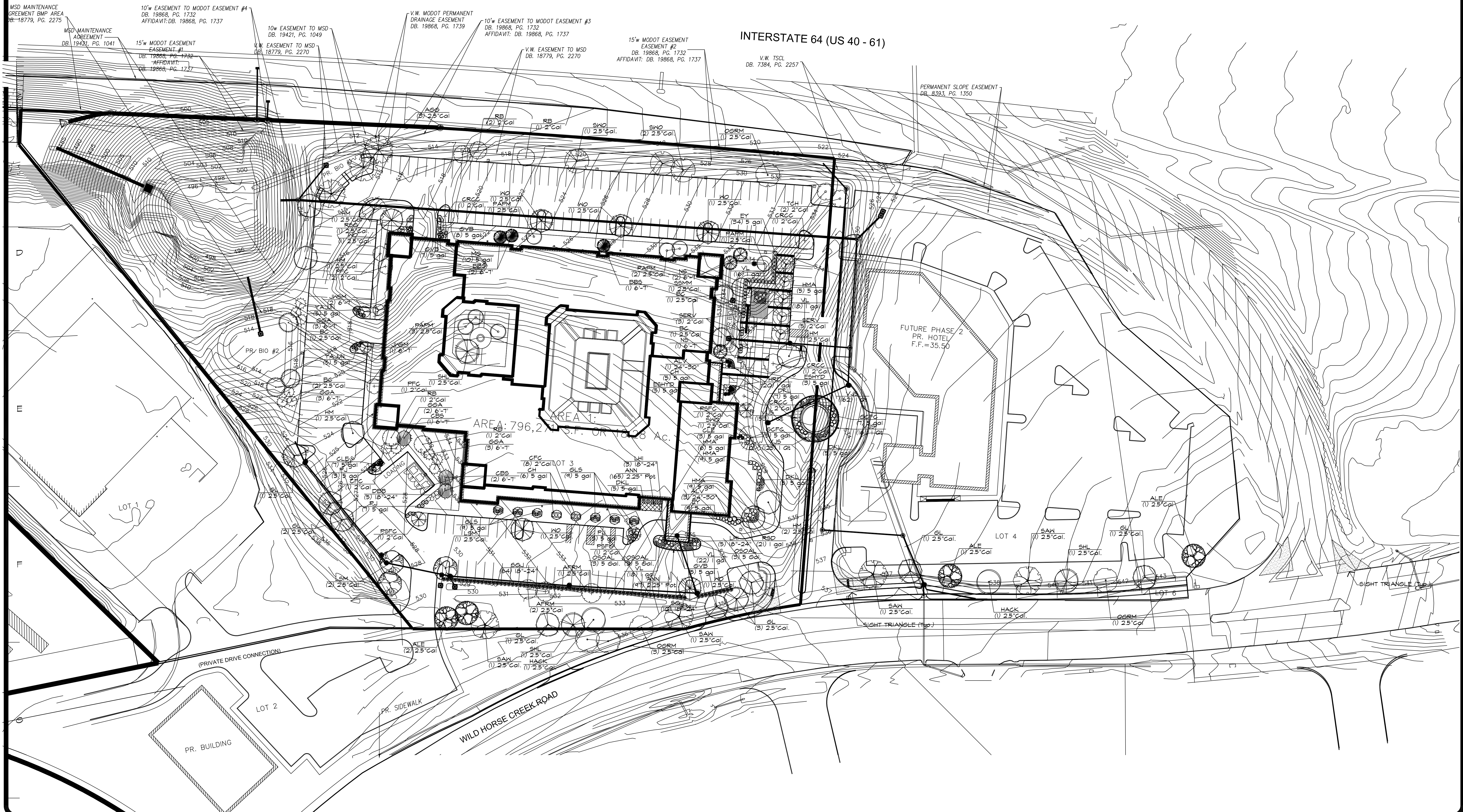
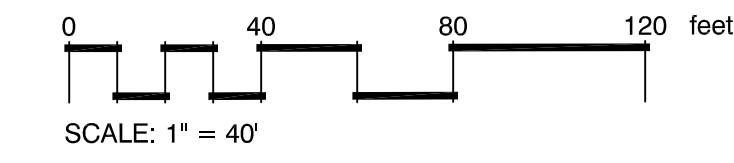
• PARKING / DRIVE AREAS: 91,110 SF @ LOT A and 76,754 SF @ LOT B

PLEASE NOTE:

- ALL LANDSCAPE AREAS & ISLANDS SHALL BE PROVIDED WITH A MECHANICAL IN-GROUND IRRIGATION SYSTEM (BY OTHERS). COORDINATE LANDSCAPING WITH IRRIGATION CONTRACTOR.
- ALL PLANTING BEDS TO BE EDGED W/ SPADE-CUT EDGE UNLESS OTHERWISE NOTED.
- ADJUST TREE LOCATIONS FOR LIGHT STANDARDS AND UNDERGROUND UTILITIES.
- NO TREES OR OTHER OBSTRUCTIONS SHALL BE LOCATED WITHIN 6 FEET OF FIRE HYDRANTS.
- ALL SHRUBS/PERENNIALS WITHIN SIGHT TRIANGLE ZONES TO BE MAINTAINED AT A MAXIMUM HEIGHT OF TWENTY FOUR INCHES (2 FEET); ALL TREES TO BE MAINTAINED WITH A CLEAR HEIGHT FROM GRADE OF TEN (10) FEET.

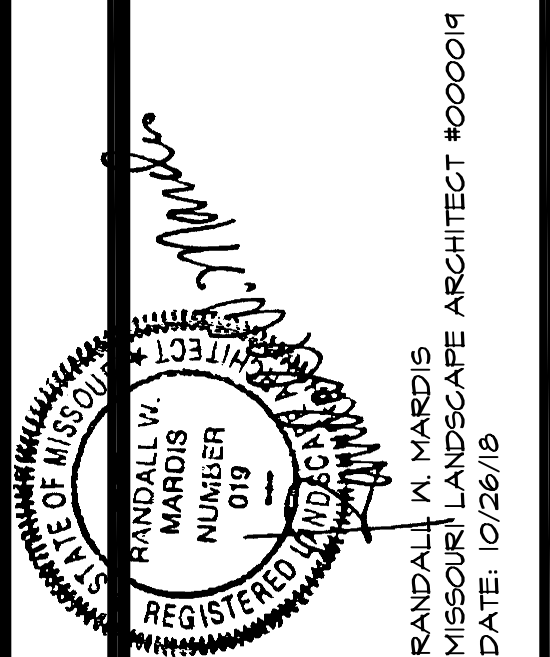


1-800-DIG-RITE



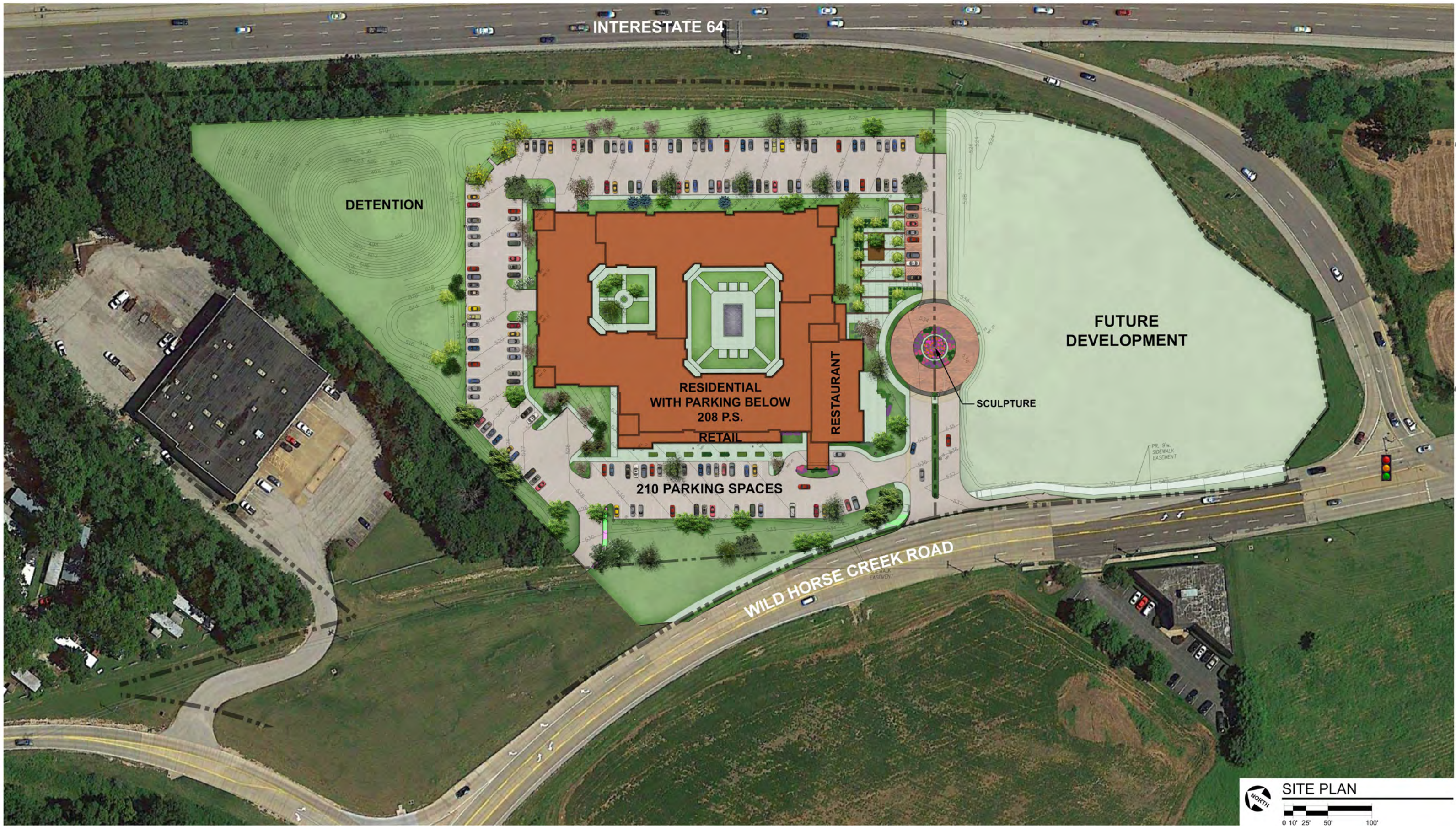
REVISIONS	BY
10/11/18	RMM
10/26/18	RMM

Landscaping TECHNOLOGIES
 67 Jacobs Creek Drive
 Chesterfield, MO 63005
 (636) 436-1250
 MISSOURI LANDSCAPE ARCHITECTURE CORPORATION #2002000192



MASTER PLANTING PLAN FOR THE PROPOSED
Wildhorse
 CHESTERFIELD, MISSOURI

DRAWN	R. MARDIS
CHECKED	RMM/VEL
DATE	9/6/18
SCALE	1"=40'-0"
JOB NO.	2018-155
SHEET	L-1
OF TWO SHEETS	



INTERSTATE 64

DETECTION

FUTURE DEVELOPMENT

RESIDENTIAL WITH PARKING BELOW 208 P.S.

RESTAURANT

RETAIL

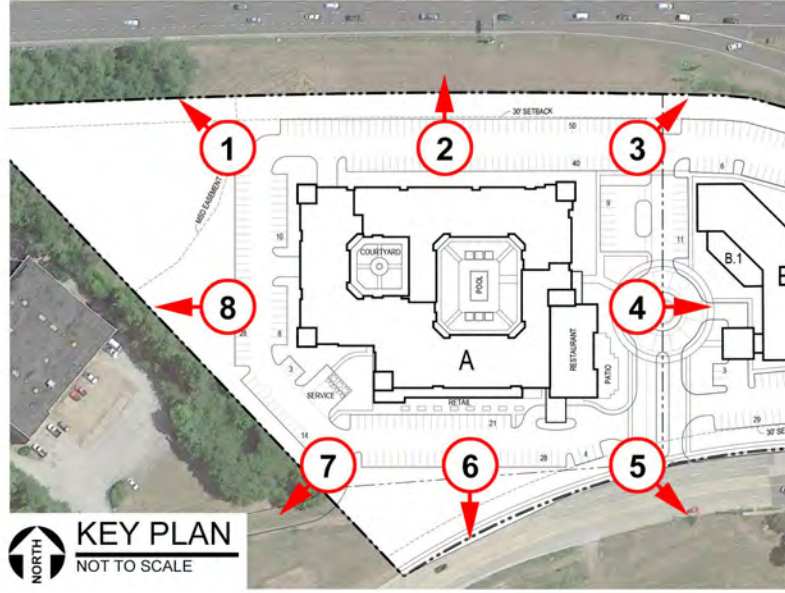
SCULPTURE

210 PARKING SPACES

WILD HORSE CREEK ROAD

SITE PLAN

0' 10' 25' 50' 100'





BUILDING PERSPECTIVE



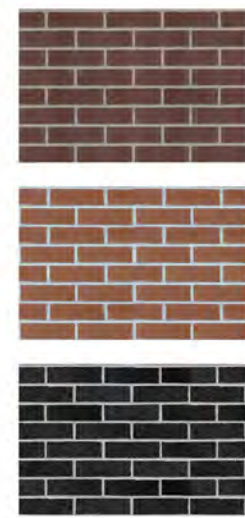
SOUTH ELEVATION
0 5' 10' 20' 40'



EAST ELEVATION
0 5' 10' 20' 40'



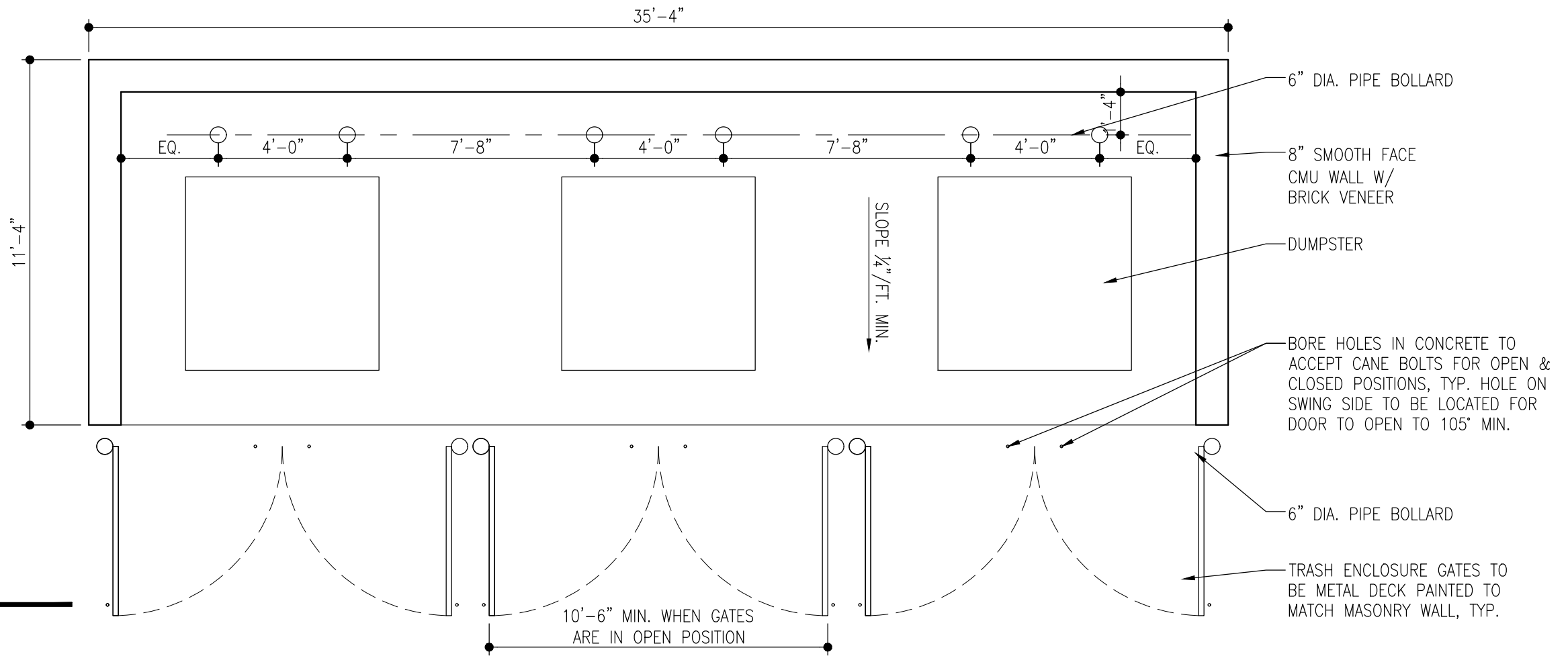
NORTH ELEVATION
0 5' 10' 20' 40'



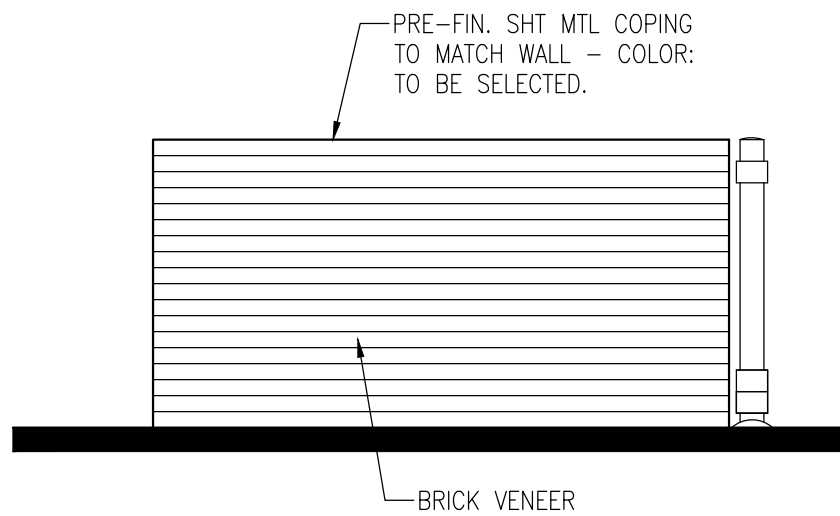
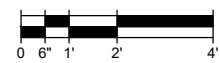
3 BRICK TYPES



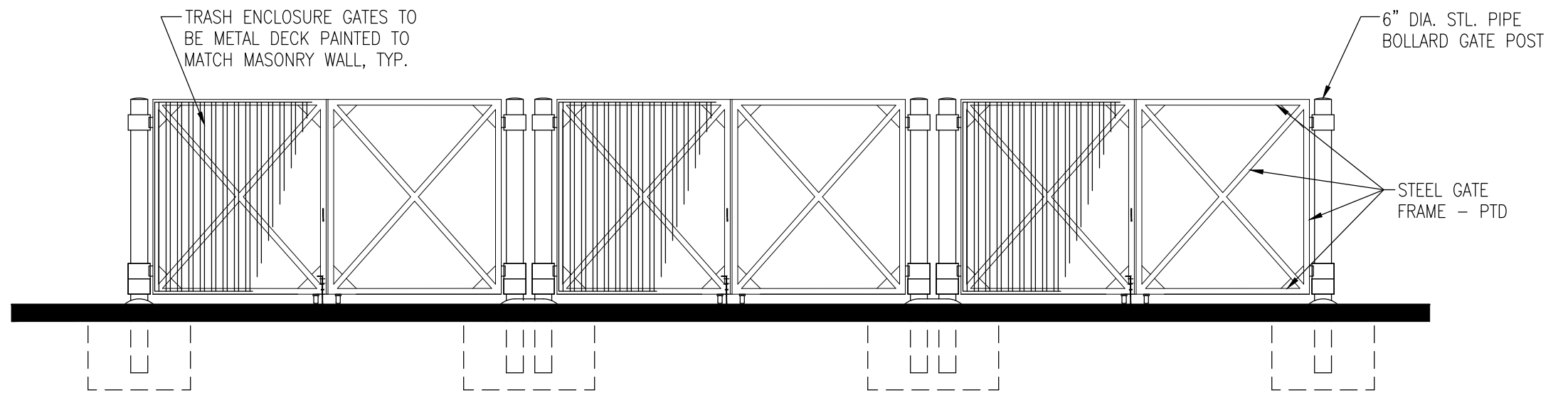
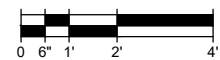
WEST ELEVATION
0 5' 10' 20' 40'



TRASH ENCLOSURE PLAN

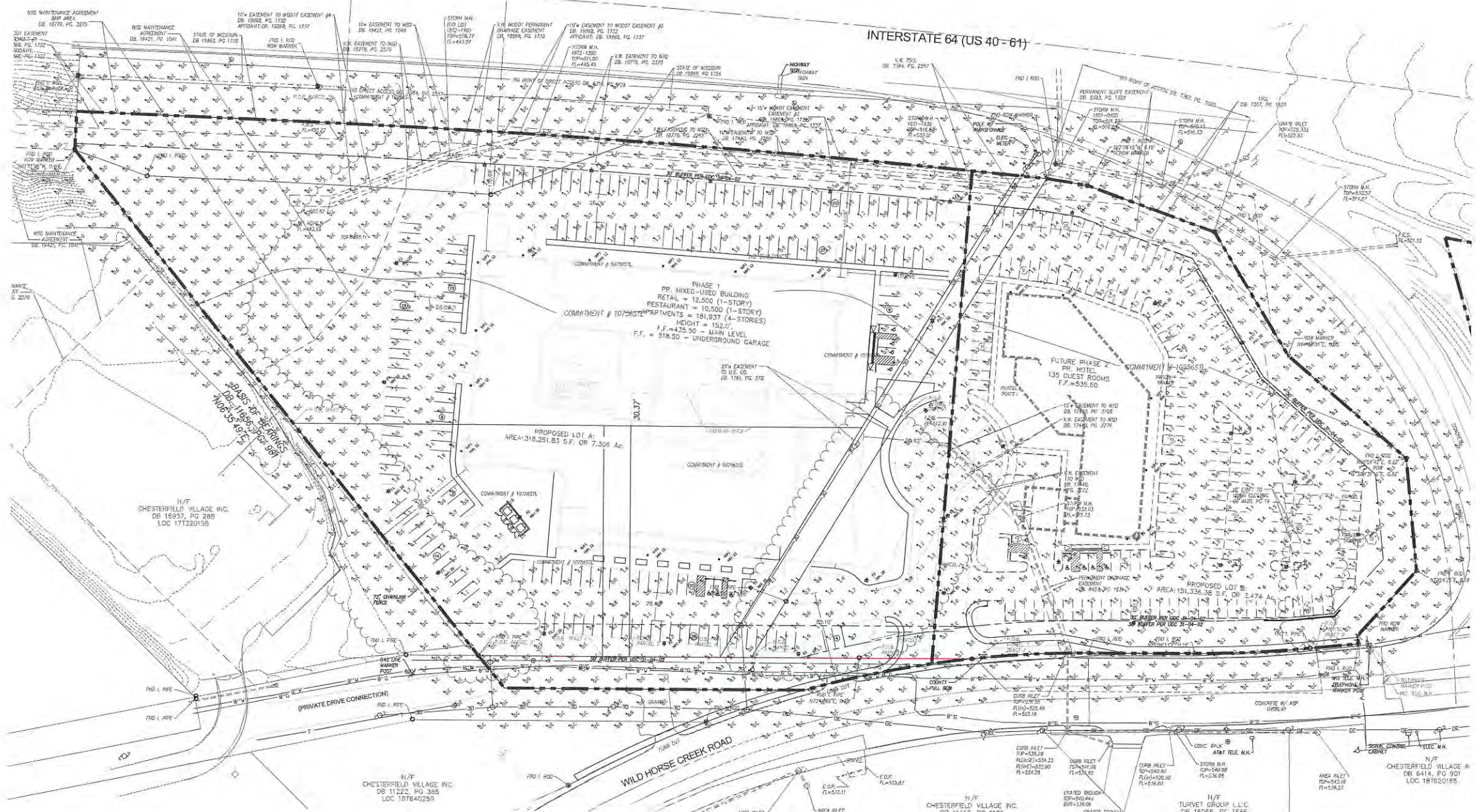


SIDE ELEVATION (TYPICAL)



FRONT ELEVATION



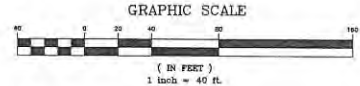


POLE FIXTURE MOUNTING HEIGHT INCLUDES BASE
 LIGHT LEVEL CALCULATED ON THE GROUND

Calculation Summary						
Label	CalcType	Units	Avg	Max	Min	Avg/Min
SITE	Illuminance	Fc	1.20	3.4	0.5	2.40
SPILL LIGHT	Illuminance	Fc	0.03	1.0	0.0	N.A.

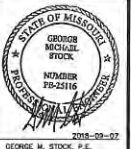
Luminaire Schedule							
Symbol	Qty	Label	Arrangement	Lum. Watts	Total Watts	LLF	Description
+	3	F1	SINGLE	59	177	1.000	GLEON-AF-01-LED-E1-SL4-HSS
□	18	WP1	SINGLE	44	792	0.912	GWC-AF-01-LED-E1-SL4-800
+	17	F2	SINGLE	59	1003	1.000	GLEON-AF-01-LED-E1-SL3-HSS
+	11	F3	SINGLE	85	935	1.000	GLEON-AF-02-LED-E1-SWQ-800

DESIGN IS BASED ON CURRENT INFORMATION PROVIDED AT THE TIME OF REQUEST.
 ANY CHANGES IN MOUNTING HEIGHT OR LOCATION, LAMP WATTAGE, LAMP TYPE, AND
 EXISTING FIELD CONDITIONS, THAT AFFECT ANY OF THE PREVIOUSLY MENTIONED, WILL
 VOID CURRENT LAYOUT AND REQUIRE A CHANGE REQUEST AND RECALCULATION.



PREPARED BY:
STOCK & ASSOCIATES
 Consulting Engineers, Inc.
 257 Chesterfield Business Parkway
 Chesterfield, MO 63005
 Phone: 636.441.8888
 Fax: 636.441.8889
 Website: www.stockandassociates.com

SITE DEVELOPMENT SECTION PLAN FOR:
THE PEARL AT WILD HORSE CREEK
 16461, 16481, 16489, 16495, 16503, 165011 WILD HORSE CREEK ROAD
 CHESTERFIELD, MO 63005 & 63017



REVISIONS:

DATE: 09/27/18
 DRAWN BY: J.S.S./A.E.B.
 CHECKED BY: G.M.S.
 SCALE: AS SHOWN
 SHEET NO.: SDSP-2

DESCRIPTION
The Galeon™ LED luminaire delivers exceptional performance in a highly scalable, low-profile design. Patented, high-efficiency AccuLED Optics™ system provides uniform and energy-conscious illumination to walkways, parking lots, roadways, building areas and security lighting applications. IP66 rated and UL Listed for wet locations.

SPECIFICATION FEATURES

Construction
Extruded aluminum driver enclosure thermally isolated from Light Squares for optimal thermal performance. Heavy-wall, die-cast aluminum end caps enclose housing and die-cast aluminum heat sinks. A unique, patented interlocking housing and heat sink provides scalability with superior structural rigidity. 3G vibration tested and rated. Optional tool-less hardware available for ease of entry into electrical cabinets. Housing is IP66 rated.

Optics
Patented, high efficiency injection-molded AccuLED Optics technology. Optics are precisely designed to shape the distribution maximizing efficiency and application spacing. AccuLED Optics create consistent distributions with the scalability to meet customized application requirements. Offered standard in 4000K (40-240K CCT @ CRI), Optional 3000K, 5000K, and 6000K CCT.

Electrical
LED drivers are mounted to removable tray assembly for ease of maintenance. 120-277V, 50/60Hz, 24/0V, 60Hz or 480V, 60Hz operation. 480V is compatible for use with 480V V/F systems only. Standard with 0-10V dimming. Shipped standard with Eaton proprietary circuit module designed to withstand 10KV of transient line surge. The Galeon LED luminaire is suitable for operation in -50°C to 40°C ambient environments. For applications with ambient temperatures exceeding 40°C, specify the HA (High Ambient) option. Light Squares are IP66 rated. Greater than 50% lumen maintenance expected at 50,000 hours. Available in standard 1A drive current and optional 800mA, 800mA and 1200mA drive currents (nominal).

Mounting
STANDARD ARM MOUNT. Extruded aluminum arm includes internal bolt guides allowing for easy positioning of fixture during mounting. When mounting two or more luminaires at 90° and 120° apart, the FA extended arm may be required. Refer to the

arm mounting requirement table. Round pole adapter included. For wall mounting, specify wall mount bracket option. QUICK MOUNT ARM Adapter is bolted directly to the pole. Quick mount arm slide into place on the adapter and is secured via two screws, facilitating quick and easy installation. The versatile, patent pending, quick mount arm accommodates multiple drill patterns ranging from 1-1/2" to 4-7/8". Removal of the door on the quick mount arm enables wiring of the fixture without having to access the driver compartment. A knock out enables round pole mounting.

Finish
Housing finished in super durable EGC polyester powder coat paint. 2.5 mil nominal thickness for superior protection against fade and wear. Heat sink is powder coated black. Standard housing colors include black, bronze, grey, white, dark platinum and graphite. metallic, RAL and custom color matches available.

Warranty
Five year warranty.

McGraw-Edison

Catalog #		Type	
Project		Date	
Comments			
Prepared by			



GLEON GALLEON LED

1-10 Light Squares
Solid State LED

AREA/SITE LUMINAIRE

DESCRIPTION
The Galeon™ Wall LED luminaire's appearance is complementary with the Galeon area and site luminaire bringing a modern architectural style to lighting applications. Flexible mounting options accommodate wall surfaces in both an upward and downward configuration. The Galeon family of LED products deliver exceptional performance with patented, high-efficiency AccuLED Optics™, providing uniform and energy-conscious lighting for parking lots, building and security lighting applications.

SPECIFICATION FEATURES

Construction
Driver enclosure thermally isolated from optics for optimal thermal performance. Heavy wall aluminum housing die-cast with integral external heat sinks to provide superior structural rigidity and an IP66 rated housing. Over all construction passes a 1.5G vibration test to ensure mechanical integrity. UPLIGHTING. Specify with the UPL option for inverted mount upright housing with additional protections to maintain IP rating.

Optics
Choice of thirteen patented, high-efficiency AccuLED Optics. The optics are precisely designed to shape the distribution maximizing efficiency and application spacing. AccuLED Optics create consistent distributions with the scalability to meet customized application requirements. Offered standard in 4000K (40-240K CCT @ CRI), Optional 3000K, 5000K and 6000K CCT. Greater than 50%

lumen maintenance expected at 50,000 hours. Available in standard 1A drive current and optional 1200mA, 800mA, and 800mA drive currents.

Electrical
LED drivers are mounted for ease of maintenance. 120-277V, 50/60Hz, 24/0V, 60Hz operation. 480V is compatible for use with 480V V/F systems only. Drivers are provided standard with 0-10V dimming. An optional Eaton proprietary surge protection module is available and designed to withstand 10KV of transient line surge. The Galeon Wall LED luminaire is suitable for operation in -50°C to 40°C ambient environments. For applications with ambient temperatures exceeding 40°C, specify the HA (High Ambient) option. Emergency egress options for -50°C ambient environments and occupancy sensor available.

Mounting
Stainless and zinc-plated mild steel mounting attachment fits directly to 4" j-box or wall with the Galeon Wall "Hook-N-Lock" mechanism for quick installation. Secured with two captive corrosion resistant black oxide-coated allen head set screws which are concealed but accessible from bottom of fixture.

Finish
Housing finished in super durable EGC polyester powder coat paint. 2.5 mil nominal thickness for superior protection against fade and wear. Standard colors include black, bronze, grey, white, dark platinum and graphite. metallic, RAL and custom color matches available. Consult the McGraw-Edison Architectural Colors brochure for the complete selection.

Warranty
Five year warranty.

McGraw-Edison

Catalog #		Type	
Project		Date	
Comments			
Prepared by			

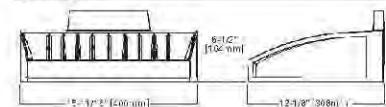


GWC GALLEON WALL

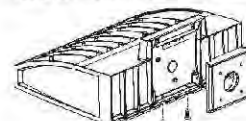
1-2 Light Squares
Solid State LED

WALL MOUNT LUMINAIRE

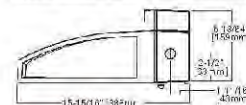
DIMENSIONS



HOOK-N-LOCK MOUNTING



BATTERY BACKUP AND THRU-BRANCH BACK BOX



CERTIFICATION DATA
JULY 1, 2014 (Date of Issue)
50000
247W/LM60 Comp art
38V max. op. Rate
100 listed
247W/247W/247W/247W
Optical

ENERGY DATA
Electronic LED Driver
40% Power Factor
~20% Total Harmonic Distortion
120V/277V/50/60Hz
307W/810W/540W
48°C Max. Temperature
40°C Max. Temperature (HA Option)
90°C Max. Temperature (HA Option)



©2014 Eaton
2014-06-26 11:31:55



CERTIFICATION DATA
JULY 1, 2014 (Date of Issue)
LM227LM60 Comp art
38V max. op. Rate
100 listed
247W/247W/247W/247W
Optical

ENERGY DATA
Electronic LED Driver
40% Power Factor
~20% Total Harmonic Distortion
120V/277V/50/60Hz
307W/810W/540W
48°C Max. Temperature
40°C Max. Temperature (HA Option)
90°C Max. Temperature (HA Option)

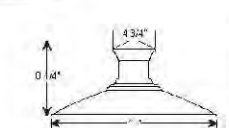
SHIPPING DATA
Approximate Net Weight:
57 lbs (25.7 kgs)



©2014 Eaton
2014-06-26 11:31:55



Specifications MCT18



Project: _____
Fixture Type: _____ Quantity: _____
Customer: _____

Specifications

Material: ALUMINUM housing, injection molded heavy duty polycarbonate lens, stainless steel mounting hardware. All fasteners are stainless steel. Inside of shade is reflective white. An anti-glare and glare shield is provided for glare reduction. UL listed for wet locations.

Electrical: 120-277V, 50/60Hz, 24/0V, 60Hz operation. 480V is compatible for use with 480V V/F systems only. Drivers are provided standard with 0-10V dimming. An optional Eaton proprietary surge protection module is available and designed to withstand 10KV of transient line surge. The MCT18 LED luminaire is suitable for operation in -50°C to 40°C ambient environments. For applications with ambient temperatures exceeding 40°C, specify the HA (High Ambient) option. Emergency egress options for -50°C ambient environments and occupancy sensor available.

Mounting: Stainless and zinc-plated mild steel mounting attachment fits directly to 4" j-box or wall with the MCT18 "Hook-N-Lock" mechanism for quick installation. Secured with two captive corrosion resistant black oxide-coated allen head set screws which are concealed but accessible from bottom of fixture.

Finish: Housing finished in super durable EGC polyester powder coat paint. 2.5 mil nominal thickness for superior protection against fade and wear. Standard colors include black, bronze, grey, white, dark platinum and graphite. metallic, RAL and custom color matches available. Consult the McGraw-Edison Architectural Colors brochure for the complete selection.

Warranty: Five year warranty.

Weight: 2.1 lbs

Category: **MCT18**, M024LD, D, W, 40K, RTC, UNV, E6, PC, 41

Catalog Number: **MCT18**

1 LIGHT SOURCE & VOLTAGES	2 DRIVER HOUSING	3 MOUNTING SOURCES Continued
<p>6124 (500W, 120V, 60Hz)</p> <p>M012D (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>M080D (80W, 80V, 60Hz)</p> <p>M018D (18W, 120V, 60Hz)</p> <p>M018LD (18W, 240V, 60Hz)</p> <p>M021D (21W, 240V, 60Hz)</p>	<p>RTC (Driver Canopy)</p> <p>RTCNC (Driver Canopy No Span Cover)</p> <p>HA (40-80°C Max. Ambient Temp)</p> <p>VOLTAGE</p> <p>120V (120-277V)</p>	<p>7 MOUNTING SOURCES</p> <p>Arm Mounts (Each Backplate Included)</p> <p>E3 E4 E6 E7 E8 E9</p> <p>E10 E11 E12 E13</p> <p>E18 E19 E25</p> <p>(See Step 2 for Driver Mounting Details)</p> <p>Wall Mounts</p> <p>WM18</p> <p>WM17</p> <p>WM16</p> <p>WM15</p> <p>WM14</p> <p>WM13</p> <p>WM12</p> <p>WM11</p> <p>WM10</p> <p>WM9</p> <p>WM8</p> <p>WM7</p> <p>WM6</p> <p>WM5</p> <p>WM4</p> <p>WM3</p> <p>WM2</p> <p>WM1</p>
4 DIMMING	5 DISTRIBUTION	6 ACCESSORIES
<p>0 (Off)</p> <p>1 (On)</p> <p>2 (On)</p> <p>3 (On)</p> <p>4 (On)</p> <p>5 (On)</p> <p>6 (On)</p> <p>7 (On)</p> <p>8 (On)</p> <p>9 (On)</p> <p>10 (On)</p> <p>11 (On)</p> <p>12 (On)</p> <p>13 (On)</p> <p>14 (On)</p> <p>15 (On)</p> <p>16 (On)</p> <p>17 (On)</p> <p>18 (On)</p> <p>19 (On)</p> <p>20 (On)</p> <p>21 (On)</p> <p>22 (On)</p> <p>23 (On)</p> <p>24 (On)</p> <p>25 (On)</p> <p>26 (On)</p> <p>27 (On)</p> <p>28 (On)</p> <p>29 (On)</p> <p>30 (On)</p> <p>31 (On)</p> <p>32 (On)</p> <p>33 (On)</p> <p>34 (On)</p> <p>35 (On)</p> <p>36 (On)</p> <p>37 (On)</p> <p>38 (On)</p> <p>39 (On)</p> <p>40 (On)</p> <p>41 (On)</p> <p>42 (On)</p> <p>43 (On)</p> <p>44 (On)</p> <p>45 (On)</p> <p>46 (On)</p> <p>47 (On)</p> <p>48 (On)</p> <p>49 (On)</p> <p>50 (On)</p> <p>51 (On)</p> <p>52 (On)</p> <p>53 (On)</p> <p>54 (On)</p> <p>55 (On)</p> <p>56 (On)</p> <p>57 (On)</p> <p>58 (On)</p> <p>59 (On)</p> <p>60 (On)</p> <p>61 (On)</p> <p>62 (On)</p> <p>63 (On)</p> <p>64 (On)</p> <p>65 (On)</p> <p>66 (On)</p> <p>67 (On)</p> <p>68 (On)</p> <p>69 (On)</p> <p>70 (On)</p> <p>71 (On)</p> <p>72 (On)</p> <p>73 (On)</p> <p>74 (On)</p> <p>75 (On)</p> <p>76 (On)</p> <p>77 (On)</p> <p>78 (On)</p> <p>79 (On)</p> <p>80 (On)</p> <p>81 (On)</p> <p>82 (On)</p> <p>83 (On)</p> <p>84 (On)</p> <p>85 (On)</p> <p>86 (On)</p> <p>87 (On)</p> <p>88 (On)</p> <p>89 (On)</p> <p>90 (On)</p> <p>91 (On)</p> <p>92 (On)</p> <p>93 (On)</p> <p>94 (On)</p> <p>95 (On)</p> <p>96 (On)</p> <p>97 (On)</p> <p>98 (On)</p> <p>99 (On)</p> <p>100 (On)</p>	<p>W (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>N (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>120V is dimming only, select "0".</p> <p>See page 2 for dimming protocols and limitations.</p>	<p>8 ACCESSORIES</p> <p>80 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>81 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>82 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>83 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>84 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>85 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>86 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>87 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>88 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>89 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>90 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>91 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>92 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>93 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>94 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>95 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>96 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>97 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>98 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>99 (120-277V, 50/60Hz, 24/0V, 60Hz)</p> <p>100 (120-277V, 50/60Hz, 24/0V, 60Hz)</p>

FIXTURE - F1, F2, F3

FIXTURE - WP1

DECORATIVE LIGHTING



CATEGORY STANDARDS

CATEGORY C - Highway Frontage Outlots (Portion of District north of relocated Wild Horse Creek Road)

Building placement:

- Build-to Line location: as defined by the streetscape shown in Exhibit 1.
- Space between buildings: 15-30 feet if detached.

Building volume:

- Minimum building height: 25 feet.
- Maximum building height: 150 feet.

Building floor heights:

- First story height shall be 12-25 feet.
- Upper story height shall be 8-18 feet.

Projecting facade elements:

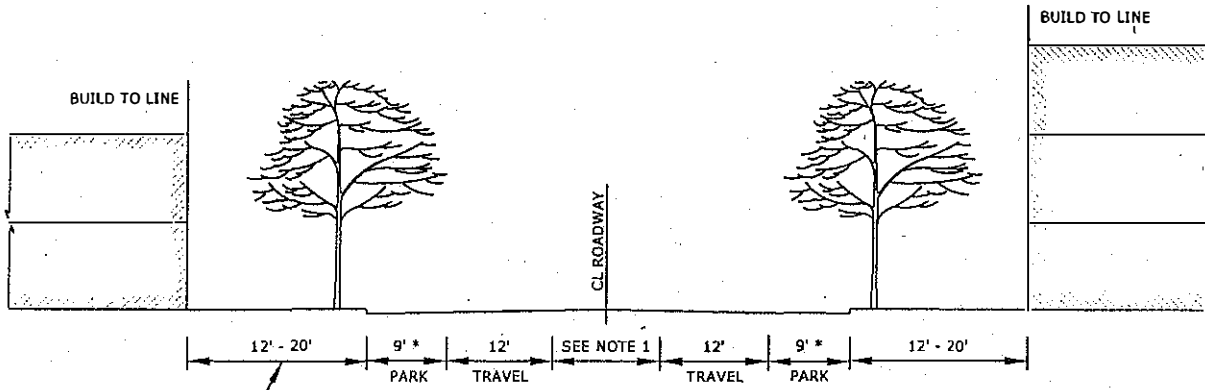
- Awnings & canopies: permitted
- Balconies: permitted
- Colonnades & arcades: permitted
- Stoops: prohibited
- Front porches: prohibited
- Bay windows: permitted on upper stories

Outdoor space:

- Courtyard: permitted (open to public if next to sidewalk)
- Frontyard: permitted
- Rearyard: permitted
- Sideyard: permitted

Building function:

- Retail commercial: permitted
- Office commercial: permitted
- Residential: permitted
- Civic: permitted
- Lodging: permitted
- Parking Structures: permitted - ground floor retail commercial or office commercial required along street frontage
- Park & Recreation: permitted



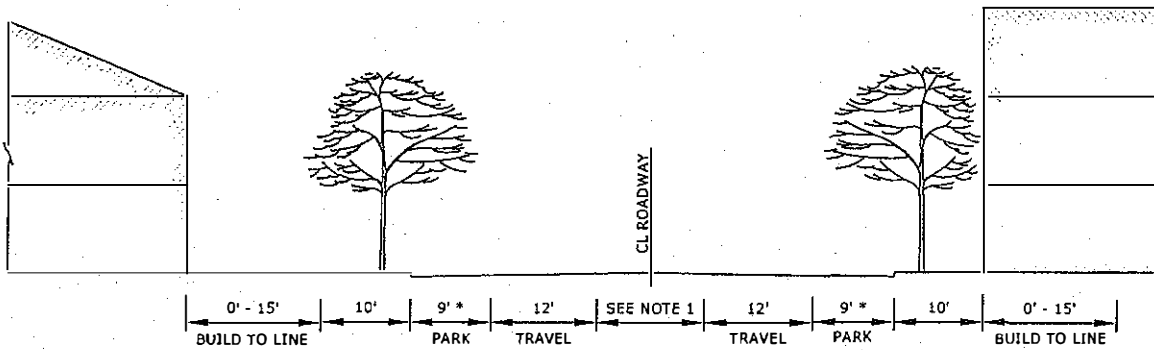
MAY BE VARIED WITH SPECIAL CONDITIONS AS APPROVED BY THE DEPT. OF PLANNING AND PUBLIC WORKS.

* CAN BE UP TO 19' IF PERPENDICULAR PARKING OR DIAGONAL OPTION, OR, ZERO IF NO ON STREET PARKING

NOTES:

1. OPTION OF ADDITIONAL LANES OR MEDIAN

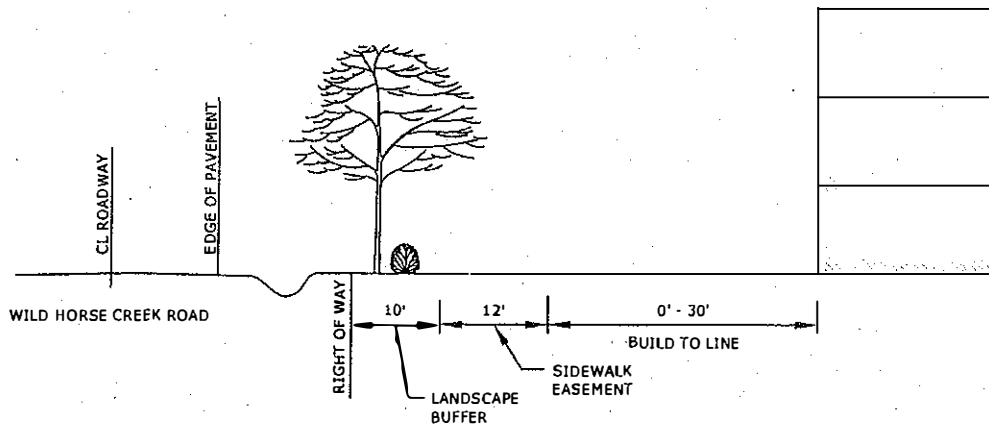
URBAN MAIN STREET



NOTES:

1. OPTION OF ADDITIONAL LANES OR MEDIAN

URBAN MIXED USE



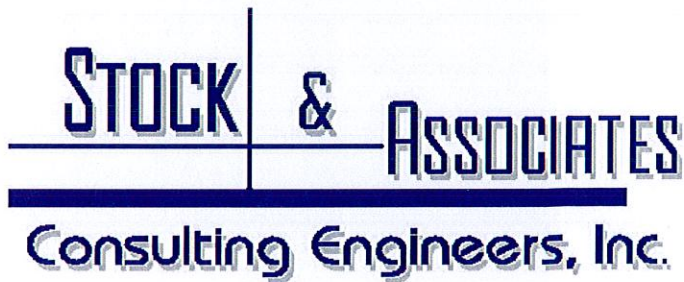
HIGHWAY FRONTAGE OUTLOTS



EXHIBIT 1

Department of
Planning and
Public Works





October 29, 2018
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

Attention: Mr. Mike Knight, Planner

RE: Downtown Chesterfield (The Pearl at WHC) – Highway Frontage Outlots – Design Intent (Stock Project No. 218-6283.4)

Dear Mr. Knight:

Pursuant to your Site Development Section Plan Comment letter dated September 26, 2018, in particular “General Comments” 7 & “Site Development Section Plan (SDSP)” comment 28 “A-D.” we respectfully request your consideration to modify Exhibit 1 of City of Chesterfield Ordinance 2449 to create Design Standards for Category C - “Highway Frontage Outlots” (Portions of District north of relocated Wildhorse Creek Road).

Comment 7. Section I.O. of the governing ordinance states that upon application from a petitioner, any performance standard provided in the Attachment “A” or required by any other District regulation or Ordinance of the City may be supplemented, modified, or altered provided such supplement, modification, or alteration will further the purpose and intent of the PC&R District. A public hearing is not required in the process and recommendation by planning commission will be forwarded to City Council for final approval. If modification is requested, please submit a separate request and dictate how this request will further the purposed and intent of the PC&R district.

Comment 28. The governing ordinance has specific design guidelines for Category C. Below are conflicts found within Exhibit 1 of the governing ordinance and the SDSP submitted. Amend plan to conform or if the applicant wishes to deviate from this exhibit, please follow steps outlined in Comment #7 of this letter.

- A. Current plan states a 30’ landscape buffer along WHCR, Exhibit 1 has a maximum of 10’.*
- B. Depict the 12’ sidewalk easement required in Exhibit 1.*
- C. The maximum distance a building can be from the WHCR right of way is 52’ according to Exhibit 1. The minimum distance of a building from WHCR is no less than 100’ represented on the current submittal.*
- D. Exhibit 1 specifically removes parking between WHCR and the “build to” line unlike Categories A & B. This submittal has 2 rows of parking between WHCR and the developments on Lots A & B. Adjust accordingly.*

The intent of our request is to achieve the goals & objectives of the “PC&R” District. The streetscape ‘Design Intent’ of Exhibit 1 is provided internally within the development. We are proposing a thirty (30) landscape buffer along Wildhorse Creek Road in place of a ten (10) foot buffer as depicted in Exhibit 1 to be consistent with the other developments along Wildhorse Creek Road. Due to the geometry of the Wildhorse Creek Road right-of-way, pavement curvature and existing utilities, we are proposing an eight (8) foot walk within a nine (9) foot wide easement, it will run along Wildhorse Creek Road in the thirty (30) foot wide landscape buffer. This sidewalk width is

**257 CHESTERFIELD BUSINESS PARKWAY • ST. LOUIS, MO 63005 • (636) 530-9100
Fax (636) 530-9130 • E-MAIL ADDRESS: general@stockassoc.com**

consistent with the developments to the east & west and in placing the sidewalk closer to Wildhorse Creek Road it would promote circulation to the future developments of the adjacent properties comprising the "Downtown Chesterfield" district in addition to providing the opportunity for pedestrians using the sidewalks along West Chesterfield Parkway access to the development.

In the center of the proposed development would be a plaza, comprised of a round-a-bout, public art, sidewalks, and park benches. This would be the focal point as one enters the development. All vehicles entering the development would navigate through the round-a-bout, similar to Carondelet Plaza in Clayton, Missouri. The proposed buildings will surround the plaza and bring a "downtown" feel to the area and further the intent of the PC&R by promoting efficient and effective pedestrian and vehicle circulation. Parking for the development will be both surface and below grade garage parking. Residents would utilize the garage while the surface parking would be used by patrons of the restaurant and boutique retail. A valet service will also be a component of the restaurant.

We respectfully request your consideration of this modification request. Please let us know if any additional information is required to complete your review of this matter.

As always, we greatly appreciated your consideration & continuous cooperation.

Sincerely,

George M. Stock

George M. Stock, P.E.
President

CC: Jeff Tegethoff – Pearl Capital Management
Brad Richey – Pearl Capital Management
Ryan Rans – Great Lakes Capital
Mary Machon – Brinkmann Holdings LLC
Josh Barcus, P.E., Associate
Kate Stock Gitto, E.I., Project Engineer