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### **Architectural Review Board Staff Report**

Meeting Date: August 12, 2021

From: Chris Dietz, Planner

**Location:** 18481 Outlet Blvd.

Description: Chesterfield Blue Valley, Lot 1G SDSP: A Site Development Section Plan,

Landscape Plan, Lighting Plan, Architectural Elevations and Architectural Statement of Design for a 4.32-acre tract of land located along the east side of

Outlet Boulevard, north of its intersection with Olive Street Road.

### **PROPOSAL SUMMARY**

Stock and Associates, on behalf of Echo Park, has submitted a Site Development Section Plan, Landscape Plan, Lighting Plan, Architectural Elevations and Architectural Statement of Design for Lot 1G of the Chesterfield Blue Valley subdivision. The purpose of this request is to convert a vacant lot to an inventory parking area and car wash in conjunction with the repurposing of Lot 1H.

### **HISTORY OF SUBJECT SITE**

2006—Site was rezoned from "NU" Non-Urban District to "PC" Planned Commercial District.

2008—Initial Site Development Concept Plan was approved for this development.

2013—Overall Design Package for the Chesterfield Blue Valley Development was approved by the Planning Commission, as required by the governing ordinance.



Figure 1: Subject Site Aerial

### **STAFF ANALYSIS**

There are three (3) sets of criteria by which this development should be evaluated, as all three pertain to this development. In order from general to most specific, these requirements are:

- UDC Architectural Review Design Standards
- Chesterfield Valley Development Requirements
- Chesterfield Blue Valley Design Standards

These sets of criteria, as they pertain to this project, will be discussed in further detail below.

### **UDC Architectural Review Design Standards:**

The Unified Development Code's Architectural Review Design Standards are broken down into two (2) areas of review: Site Design and Building Design.

General Requirements for Site Design are further broken down into the following categories:

- Site Relationship
- Circulation and Access

- Topography and Parking
- Retaining Walls (Not applicable)

General Requirements for Building Design are also broken down into the following categories:

- Scale
- Design
- Materials and Color

- Landscape Design and Screening
- Signage
- Lighting

The UDC also includes specific site and building design criteria for commercial and industrial architecture, as well as shopping center and office complex uses, shown in Table 1 below:

	Access	Exterior Elements	Landscaping and Screening	Scale	Site Design
Commercial and Industrial Architecture	Locate service and loading areas away from public streets and out of the main circulation system and parking areas. Provide access for service vehicles, trash collection and storage areas from alleys when possible. If not possible, utilize the street with the least traffic volume and visual impact.	See Section 405.04.010(D), General Requirements For Building Design, of this Article.	Screen utility meters and surface transformer switching pads.	See Section 405.04.010(D), General Requirements For Building Design, of this Article:	Design and locate building equipment and utilities to minimize visibility from public streets, surface parking lots, and neighboring properties.

Table 1: UDC Commercial and Industrial Design Guidelines

### **General Requirements for Site Design:**

The Unified Development Code details both desirable and undesirable practices for Site Design, as shown in the table below:

Desirable Practices	Undesirable Practices
Provide safe pedestrian movement between elements	Site design that impairs or interferes with other properties or developments
Provide public plazas, courtyards, assembly areas, etc.	Excessive noise, lighting, glare
Incorporate scenic views, fountains, public art, etc., within outdoor spaces	Delivery zones, trash enclosures, storage areas, transformers and generators that are not screened and are visible by the public
Consider climate, solar angles, and outdoor activities when designing elements within outdoor spaces	Aboveground public utilities

Table 2: General Guidelines for Site Design

### **Chesterfield Valley Development Requirements:**

The Unified Development Code also details criteria for development within Chesterfield Valley. These criteria, as they pertain specifically to this development, are listed in the table below:

- Utilize architectural elements from the front façade on the side and rear of the structure.
- Screen loading areas and construct with materials consistent to the building.
- Screen outdoor storage of goods, equipment or automobiles for sale or service from I-64/US 40.
- Locate parking primarily to the side or rear of any building façade facing I-64/US 40.

### **Chesterfield Blue Valley Design Standards:**

In addition to the design standards outlined in the UDC, the governing ordinance, Ordinance 2805, has certain architectural provisions for this development. Among these provisions, the ordinance states that an overall Design Package specifically for the Chesterfield Blue Valley subdivision is required. Adopted in 2013, the goal for this Design Package is to provide "a coordinated, upscale design theme present in every architectural and landscape feature including built form, lighting, exterior furniture and landscaping". Ordinance 2805 also states that this Design Package should give consideration to creating a visually-appealing development pattern, particularly from I-64, achieved by retaining open spaces and creating consistency in the location and massing of structures on site.

The Design Package categories that apply to this request are:

- Material Palette
- Color Palette
- Lighting

- Street Furniture (Bollards)
- Landscape

This proposal's compliance with each of these sections of the design package will be discussed in detail and will appear in <u>italics</u> in their applicable sections below. A copy of this Design Package is included in the Architectural Review Board packet.

### A. Site Relationships

Lot 1G is located along the southeast boundary of Chesterfield Blue Valley development across the street from an undeveloped parcel to the southwest, a retail building to the southeast, and vacant building and parking lot on 1H to the northwest. Interstate 64 and a Stormwater Master Channel abut the northeast side of the subject site. Lot 1G remains undeveloped to this day and will serve as an accessory parking area and carwash use for inventory vehicles for the automobile sales use of Lot 1H. However, as the parking area and carwash are both permitted uses located on Lot 1G, each of these uses could eventually become standalone uses, potentially separate from the user on Lot 1H.

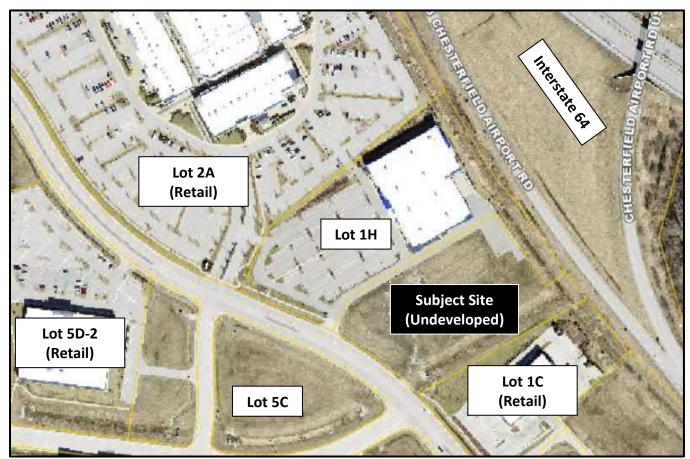


Figure 2: Site Relationships

#### **B.** Circulation and Access

The subject site has vehicular cross access between both Lot 1G and Lot 1H. This proposal would fence off most of the proposed parking area on 1G for inventory vehicle parking, with a small carwash—incidental to inventory vehicle maintenance—located on the north side of the lot. Two (2) gates for vehicular access will be constructed: one located at the southwest corner of the site, and another located to the north. Bollard fencing will be utilized to enclose the inventory parking area with approximately 220 linear feet of it running across pavement to the south and north of the carwash. This bollard fencing is shown as pink hatching in Figure 3 below. A new pedestrian sidewalk will be constructed along Outlet Boulevard connecting to the existing sidewalks on both Lots 1C (southeast) and 1H (northwest).

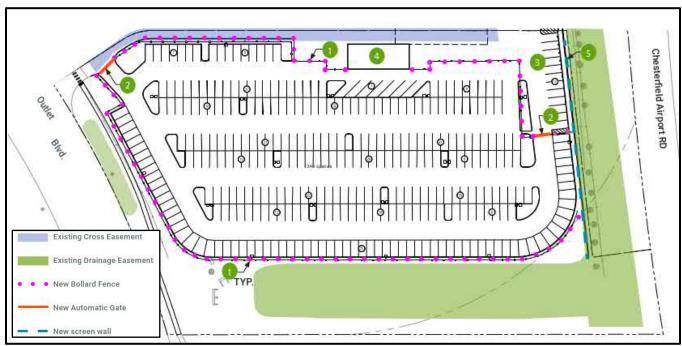


Figure 3: Color Site Plan

### C. Topography

The site is generally flat with a Stormwater Master Channel running along the east property line.

### **General Requirements for Building Design**

### A. Scale

The proposed carwash building is roughly 2,000 ft<sup>2</sup> and 15'0" in height, utilizing a small footprint in comparison to the surrounding uses. A screening wall will run along the entirety of the eastern property line, reaching a height of fifteen feet (15'0") at its highest point.

### B. Design

The carwash building will utilize a simple four-sided design with inventory vehicles entering through the south door and exiting to the north.

The parking area will feature a two-foot, six-inch (2'6") bollard fence surrounding a large portion of the site's parking area with gated access to this lot on the east and northwest sides. Notably, this bollard fencing will extend across 220 linear feet of pavement on both the north and south sides of the carwash. A detail of this bollard fencing is illustrated in Figure 4 below. A photo example of this bollard fencing is included in the ARB packet. The two (2) gates entering this inventory parking area will stand 6'0" at their highest point and will not be illuminated. A photo example of this gate and a detail are both found in the ARB packet as well.

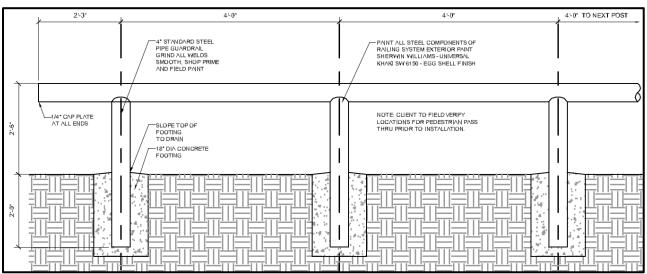


Figure 4: 2'6" Bollard Fencing Detail

Chesterfield Blue Valley's Design Package requires a prairie-style design for buildings. The proposed bollard fencing is not outlined in Chesterfield Blue Valley's Design Package and is not found elsewhere in the subdivision.

The screening wall that extends along the northeastern property line is intended to screen the carwash exit door and inventory vehicle parking from view along I-64 and the exit ramp onto Chesterfield Airport Rd., as well as the overhead doors for the building on Lot 1H. The height of this wall will begin at twelve feet (12') before rising to fifteen feet (15') in height and continuing toward Lot 1H. While this screening wall runs across both Lots 1G and 1H, Figure 5 only details the screening wall as it appears on Lot 1G. Figure 6 depicts the screening wall across both lots in its entirety.



Figure 5: Screening Wall Elevation (1G Only)

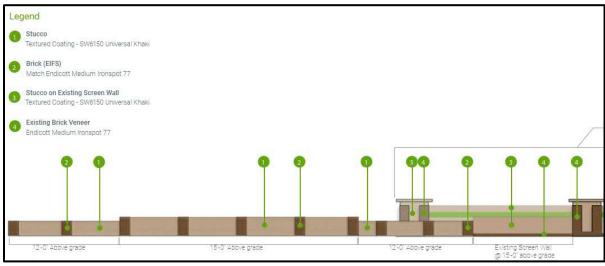


Figure 6: Screening Wall Elevation (1G & 1H)

### C. Materials and Color

The carwash and screening wall will utilize a collection of materials that closely resemble those found on the existing building on Lot 1H. The carwash elevations feature a three-foot (3') high dark brick watertable design at the base of the structure, with textured tan EIFS comprising the remainder of each elevation before being capped with grey coping at the top of the structure. The overhead doors of the carwash will be metal painted to match the tan EIFS of the structure.

The screening wall will be comprised of Concrete Masonry Units (CMU) with stucco finish and brick-style EIFS sections breaking up the design throughout. This brick-style EIFS will match the brick of the existing building on Lot 1H as well. The wall will be capped with sandstone coping.

The 2'6" high bollard fencing encircling the inventory lot will be comprised of steel, painted tan to match that proposed on Lot 1H.

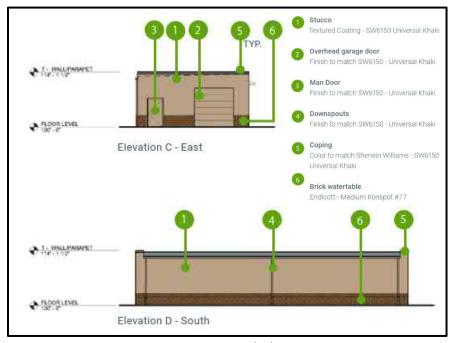


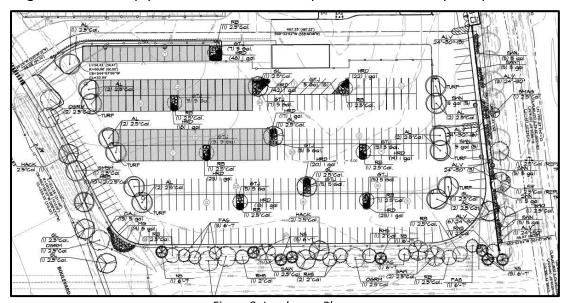
Figure 7: Carwash Elevations

The Material Palette for the Chesterfield Blue Valley Design Package calls for brick and stone veneer, EIFS, smooth face architectural metal, exposed steel structure, painted concrete wall pans with sand texture finish, open metal trellises, glass and aluminum store fronts and curtain walls, canvas and metal awnings. The color palette requires earth tones accompanied by accent colors for view corridors and arrival points.

### D. Landscape Design and Screening

A landscape buffer would typically be required along I-64 to the east of this property. However, a Concept Landscape Plan, approved as part of the 2<sup>nd</sup> Amended Site Development Concept Plan, states that an alternative landscape buffer be provided due to the location of the Stormwater Master Channel on the northeast side of the property. The applicant has proposed a mix of trees and shrubs along the screening wall on the northeast property line to soften the appearance of the wall, and continuing around the east corner before continuing along the southeast property line. Additionally, landscaping will be provided on islands throughout the parking area and along Outlet Blvd.

All planting selections comply with the Unified Development Code landscape requirements.



As noted earlier, Ordinance 2805 requires screening of outdoor sales from external roadways for this development. The UDC Architectural Review Design Standards also require the screening of loading areas and outdoor storage of automobiles from I-64. The combination of the 8'0" screening wall and landscaping along the eastern property line of 1G are intended to adequately screen the outdoor inventory parking area and carwash exit door. It is also intended to screen the overhead doors on Lot 1H from I-64 as well.

Chesterfield Blue Valley's Design Package states that Landscaping should add to the visual appeal, help in spatial definition, wayfinding and effective circulation, keeping in mind that site entries, walkways and parking fields should be well illuminated and not create isolated areas and allow clear views throughout each site.

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### E. Lighting

The parking area will include additional lighting fixtures that match those found throughout Chesterfield Blue Valley subdivision today. There will also be wall pack lighting fixtures above each pedestrian door on the carwash. These fixtures are to be flat lensed and fully shielded. No other new fixtures are being proposed with this request.

The parking lot pole mounted fixtures adhere to Chesterfield Blue Valley's Design Package, whereas the wall-mounted fixtures are not specifically outlined within it.

### F. Exterior Renderings



Figure 9: Carwash (South Entrance)



Figure 10: View from Outlet Blvd.



Figure 11: View From 1-64 Exit Ramp (Landscape Faded From View)

### **DEPARTMENT INPUT**

This project is currently under Staff review and will not proceed to the Planning Commission until all outstanding items have been addressed. All recommendations made by the Architectural Review Board will be included in Staff's report to the Planning Commission. Staff requests review and recommendation by the Architectural Review Board on the Site Development Section Plan for Chesterfield Blue Valley, Lot 1G (Echo Park). A copy of the Chesterfield Blue Valley Design Package and the applicant's submittal packet are attached to this report for the Board's consideration.

### **MOTION**

The following options are provided to the Architectural Review Board for consideration relative to this application:

- 1) "I move to forward the Site Development Section Plan, Landscape Plan, Lighting Plan, Architectural Elevations and Architectural Statement of Design for Chesterfield Blue Valley, Lot 1G (Echo Park), as presented, with a recommendation for approval (or denial) to the Planning Commission."
- 2) "I move to forward the Site Development Section Plan, Landscape Plan, Lighting Plan, Architectural Elevations and Architectural Statement of Design for Chesterfield Blue Valley, Lot 1G (Echo Park), to the Planning Commission with a recommendation for approval with the following recommendations..."

Attachments: Chesterfield Blue valley Design Package

Architectural Review Board Packet

### Introduction

Chesterfield Blue Valley (CBV) is a mixed use development at the gateway to the City of Chesterfield and St. Louis County at Chesterfield Airport Road and relocated Olive Street Road. The 1.4 million square foot project is anchored by St. Louis Premium Outlets which is scheduled to open in August 2013. CBV is a land development project of Chesterfield Blue Valley, LLC, a Missouri limited liability company (CBV, LLC). CBV, LLC's manager is Wolfe Properties, LLC, whose principal is R. Dean Wolfe. CBV, LLC's listing broker is L3 Corporation whose principals are Ian Silberman and Craig Wielansky.

The purpose of this Construction Manual is to compile in one place important information about CBV to give prospective owners and users a good starting point as to what to expect when developing a project within CBV. Please note that the information contained in this Construction Manual is a starting point only and is not intended to serve as a comprehensive compilation of all relevant documents and information. Prospective owners and users are expected and encouraged to conduct their own due diligence. CBV, LLC has attempted to be helpful by providing the information included in this Construction Manual but makes no warranties or representations as to its accuracy or completeness. Moreover, all information and documents included in this Construction Manual are subject to change due to market conditions, governmental directives or other reasons. CBV, LLC is not under any duty to advise anyone of any such changes.

### Architectural Concept Standards

The following are the architectural standards contemplated for CBV. These standards are likely to be incorporated into formal legal documents. CBV, LLC reserves the right to modify or waive these standards as market conditions may dictate. Moreover, these standards are subject to modification and supplementation by governmental authorities.

### General

CBV, LLC envisions a project design based on the character, principles and theme of the Prairie Style of Architecture which contains horizontal lines, flat or hipped roofs with broad overhanging eaves, and window groups in horizontal banks as the principle means of



architectural expression. The goal is for a coordinated, upscale design theme present in every architectural and landscape feature including built form, lighting, exterior furniture and landscaping. The historic Brasher House, if reused, would be an exception. It is not the intent of these standards to limit or diminish use of "corporate trade dress" on store fronts.

### **Material Palette**

Brick and stone veneer, exterior insulation plaster system (EIFS), smooth face architectural metal, exposed steel structure, painted concrete wall panels with sand texture finish, open metal trellises, glass and aluminum store fronts and curtain walls, canvas and metal awnings, and appropriate cornices and overhangs will be acceptable.

### Color Palette

Earth tones accompanied by complimentary accent colors for view corridors and arrival points will be the standard. Attached is the list of colors and materials used in St. Louis Premium Outlets for buyer's reference.

### Lighting

Lighting scheme will complement the architectural style creating a consistent theme and look throughout the CBV development.

- Parking Field Illumination. High mast poles and fixtures matching those on street lights throughout CBV that do not transmit light beyond the extent of each parcel throughout the parking fields shall be used. Pole and fixture color will match street light poles and fixtures in size, shape, height and color. Attached is the cut sheet for the street lights for buyer's reference. Foot candle and photo metric design shall meet City of Chesterfield standards.
- Walkway Illumination. Pedestrian scale vertical lamps should be distributed on pedestrian corridors in a complimentary style and color.
- <u>Building Illumination</u>. Wall sconces will be fixed on appropriate vertical points and at building corners. Sconces will have a complimentary style and color to other light fixtures.



### Street Furniture

The design, character and materials of benches, waste receptacles, bollards, table and chair combos, stand-alone chairs and bicycle racks should be coordinated and complete the architectural design throughout the development.

### Landscaping

Details of the landscape are critical components of the character and quality of the design.

Landscaping should add to the visual appeal, help in spatial definition, way finding and effective circulation, keeping in mind that site entries, walkways and parking fields should be well illuminated and not create isolated areas and allow clear views throughout each site.

Landscaping along the four major streets—Outlet Boulevard, Premium Way, Premium Street and Blue Valley Avenue—should be consistent with Master Landscape plans filed as part of the 2<sup>nd</sup> Amended Site Development Concept Plan. Each buyer is to be responsible for the installation of street landscaping on the frontages of each parcel to the street curb line. The City of Chesterfield requires an in-ground irrigation system for all planted areas.

### Sidewalks

Each buyer is to be responsible for installation of sidewalks on or adjacent to the parcel purchased.

### Storm Channels

Each buyer is to be responsible for the maintenance of adjacent storm water channels serving the buyer's parcel, whether or not the channels are part of the parcel purchased and are creditable as part of the 30% green space requirement of each parcel. However, CBV reserves the right to delegate this responsibility to an association to which owners contribute.

### Master Sign Plan

Signage is to be consistent with the Master Sign Plan approved by the City of Chesterfield.



### **Internal Roads**

Roads are to be defined by raised curbs, landscaping and pedestrian walkways. Colored and textured paving at locations to demarcate pedestrian routes, to calm traffic and to define entry points will be required.

### Screen Walls

Decorative screen walls are to be architectural in nature and provide a visual barrier for well-maintained utilities, service devices, compactor and loading areas.





## EchoPark - St. Louis, MO Architectural Review

Lot 1G August 12, 2021





These drawings and specifications are intended to express design intent only and do not reflect actual site conditions. Neither party shave any obligation nor liability to the other (except stated above) until a written agreement is fully executed by both parties.

These drawings and specifications are confidential and shall remain the sole property of Sonic Automotive Inc., which is the owner of the copyright in this work. They shall not be reproduced (in whole or in part), shared with third parties or used in any manner on other projects or extensions to this project without the prior written consent of Sonic Automotive Inc..

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### The EchoPark Experience

• Each retail space we develop provides a welcoming, enjoyable atmosphere with an open concept and functional zones tailored to specific activities

Interactive research areas with iPads

Comfortable lounge spaces (as opposed to closed offices)

- The environment empowers guests of EchoPark to shop the way they want at their desired pace.
- Full sensory environment ties the brand's digital and physical assets into a seamless, flexible and compelling shopping experience.
- Our design cues are derived from modern retail, hospitality and digital spaces.
- Our goal has always been to provide a guest experience that breaks the stereotype of the used car dealership. We are doing this with our people and technology infused facilities.

Respect for our guests

Comfortable environment

Vehicle inventory that meet our guest needs

Denver 2014- "I think it is important for all interested parties to note that we have been preparing for this for many years. Bringing innovative change to the industry takes experience, investment and patience. We have been working tirelessly to create a really different and amazing experience for all our guests."

- Jeff Dyke, President Sonic and EchoPark Automotive.

### **Our Retail Space - Representative Exteriors**









### **Architect's Statement of Design**

### Statement of design

This site is currently an undeveloped lot. It is the goal of Sonic to develop the empty plot into a secure, well-lit and paved guest parking and inventory storage lot.

#### **Current Conditions**

The undeveloped lot is characterized by drainage and utility easements, natural landscaping, and concrete jersey barriers.

### Proposed Site.

The currently undeveloped lot will maintain the existing site drainage and utility easements and be redeveloped as inventory parking to house high-end, gently used cars. Existing topography changes will be minor. Inventory parking will be secured by low bollard fencing and automatic gates to ensure vehicular traffic is clearly defined, efficient, safe, and secure for both guests and employees. A CMU with stucco finish screen wall and landscaping will act as a soft privacy buffer between the site and the I-64 exit ramp. This will create a more desirable view between the highway, new parking lot, and car wash. Additional or new landscaping will also be added at the perimeter of the site to serve as a sightlier transition between the new inventory parking, adjacent lot (Cavender's Western Outfitters) and Outlet Blvd. New parking lot lighting will be provided to meet the development standards, the light fixture and pole will match what has been used throughout the development area.

### Proposed design.

A detached car wash will be added to the site. This will be utilized by EchoPark to ensure all cars on site maintain a high finish standard to showcase inventory. The car wash building design will have a primary facade of stucco with a contrasting brick water table to 3 foot above grade to complement existing design within the development. All mechanical systems will be enclosed within the carwash structure, so additional screeing of mechanical units is not required.

# **Existing Site**

### **Existing Site - Location Map**

### Legend

- Closed Gander Outdoors of Chesterfield Lot 1H Zoning Classification: PC Photos: 1-4
- Undeveloped Lot Lot 1G Zoning Classification: PC Photos: 1, 4 & 5
- Cavender's Western Outfitter Zoning Classification: PC Photos: 5 & 6
- Undeveloped Lot 2 Zoning Classification: PC Photos: 7 & 9

\*\*\*Existing site photos located on pages 8





### **Existing Site Conditions - Adjacent Tenants**

### Legend

- **Closed Gander Outdoors of** Chesterfield - Lot - 1H
- Undeveloped Lot Lot 1G
- Cavender's Western Outfitter
- Undeveloped Lot 2



1. Undeveloped Lot 1 - View looking East from Outlet Blvd.



3. Undeveloped Lot 2 - View looking SW from Outlet Blvd.



2. Cavender's Western Outfitters - View looking SE from Outlet Blvd.



4. Gander Mountain - View looking SE from Outlet Blvd.

# **Proposed Site**

### **Proposed Site - Site Plan**

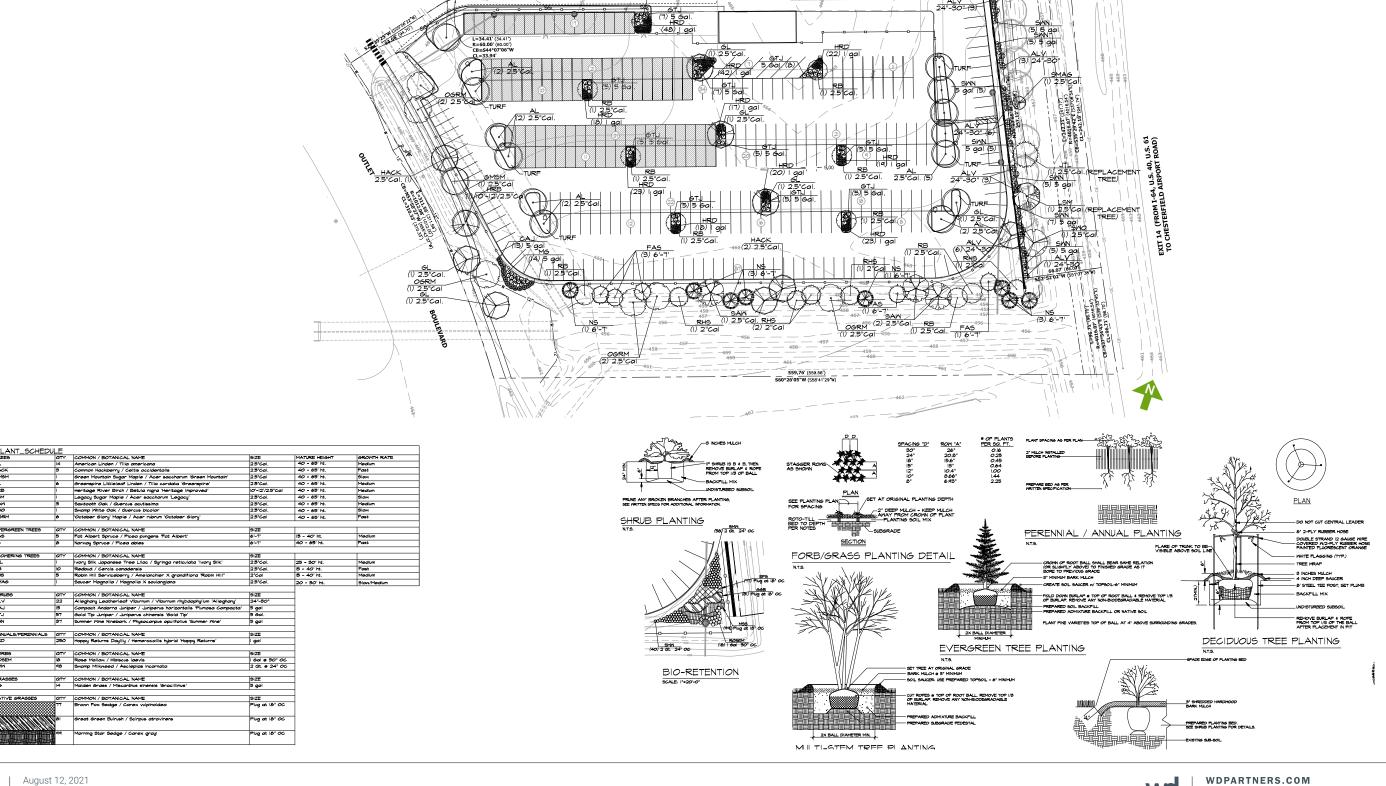
### Legend

- **Existing Cross Easement**
- **Existing Drainage Easement**
- New Bollard Fence
- **New Automatic Gate**
- New screen wall
  - New inventory bollards
  - New security gate
  - Inventory and employee parking
  - New car wash building on existing paved
  - New screen wall at various heights between 12'-0" to 15'-0" above grade.



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### **Proposed Site - Landscape Plan**



### **Proposed Site - Screen Wall Rendering**



Proposed Echo Park rendering - View looking NW fromI-64 exit ramp 14



### **Proposed Site - Topography Rendering**



Proposed Echo Park rendering - View looking NE from Outlet Blvd. with the Gander Mountain building in the background

### **Proposed Site - Car Wash Rendering**



Proposed carwash rendering



### **Proposed Site - Exterior Elevation - Carwash**

### Legend

Stucco Textured Coating - SW6150 Universal Khaki

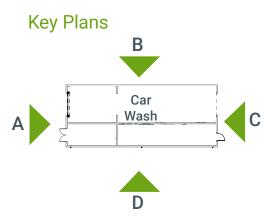
Overhead garage door Finish to match SW6150 - Universal Khaki

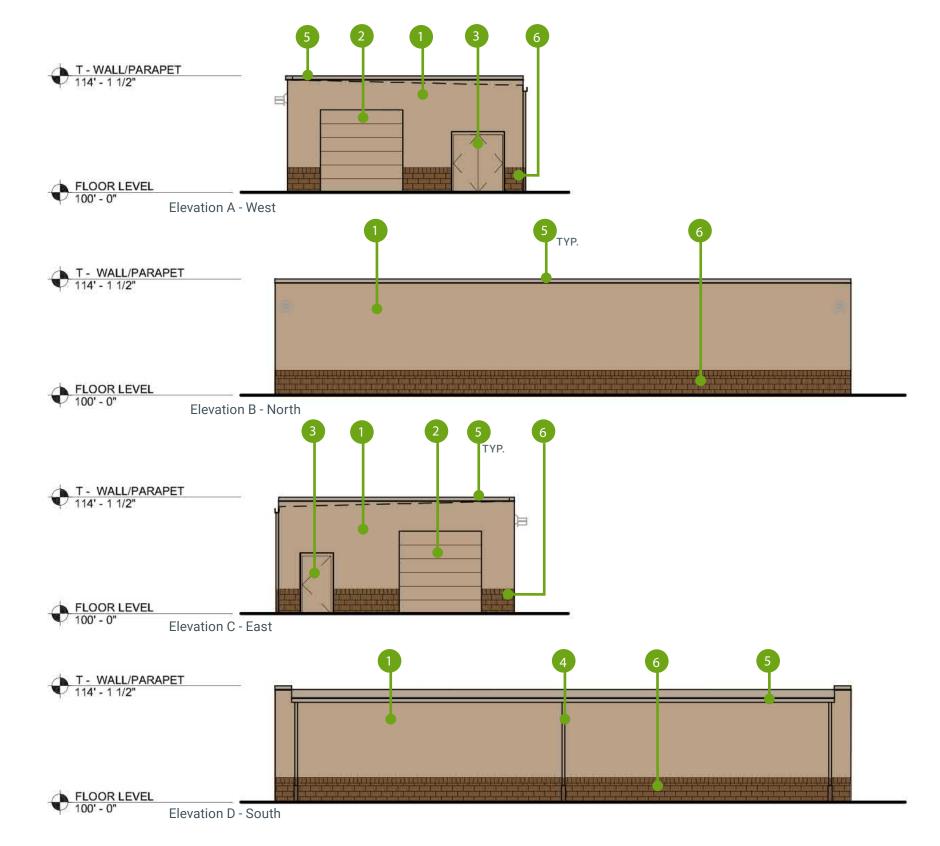
Man Door Finish to match SW6150 - Universal Khaki

**Downspouts** Finish to match SW6150 - Universal Khaki

Coping Color to match Sherwin Williams - SW6150 Universal Khaki

**Brick watertable** .Endicott - Medium Ironspot #77





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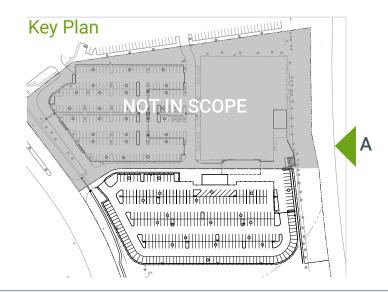
### **Proposed Site - Elevation - Screen Wall**

### Legend

- Stucco Textured Coating - SW6150 Universal Khaki
- Brick (EIFS) Match Endicott Medium Ironspot 77
- Stucco on Existing Screen Wall Textured Coating - SW6150 Universal Khaki
- **Existing Brick Veneer** -Existing Adjacent Lot H building. Endicott Medium Ironspot 77 Please refer to Lot H Submittal 12'-0" Above grade 15'-0" Above grade 12'-0" Above grade Existing Screen Wall

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Elevation A

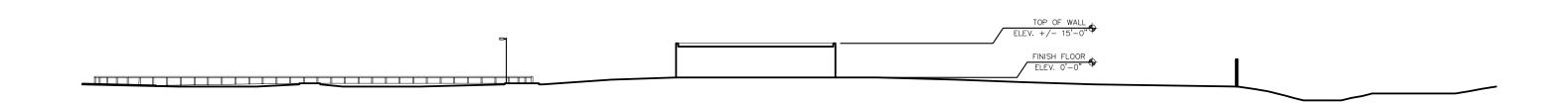


@ 15'-0" above grade

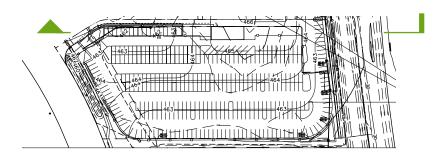
### **Proposed Site - Overall Site Section**

### **Building sections**

The new carwash structure is designed with an Earth-tone stucco finish and a warm color brick watertable is used to give the structure texture and visual interest coherent with the overall theme of the Prairie Style of Architecture.



### Key Plan



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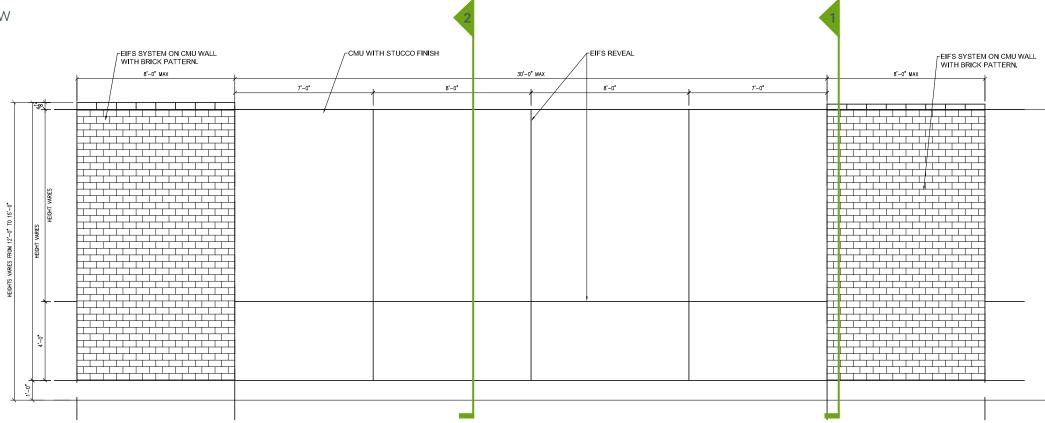
### **Proposed Site - Details**

### **Fence Screening**

Decorative fencing is to be used to obscure adjacent properties, manage sightlines and create an emotionally appealing guest experience. It is also complimenting the theme of Prairie Style of Architecture in the development area.

### Security bollard fencing and gates

In order to secure parking zones, security bollards and gates are used where needed in order to allow for quick access of vehicles to waiting guests. Bollard fencing is used to create clear vehicle circulation, protect inventory and maintain sightlines. It is to blend into the existing development and not stand out. See photos on page 23.

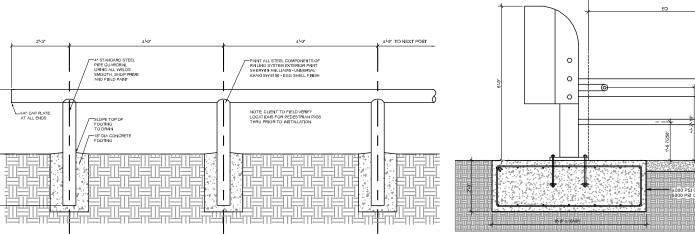


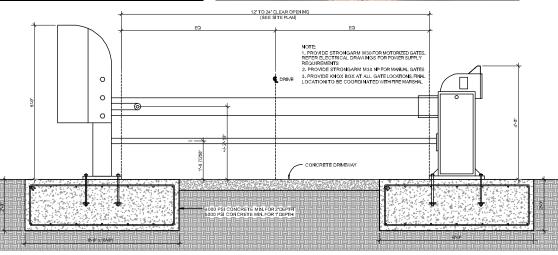
Front Elevation @ Screen wall



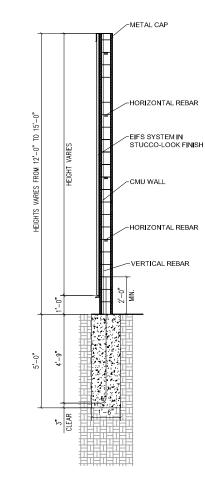












Security Bollard Fencing

Security Gate StrongArm M30 / M30

✓EIFS SYSTEM IN STUCCO-LOOK FINISH

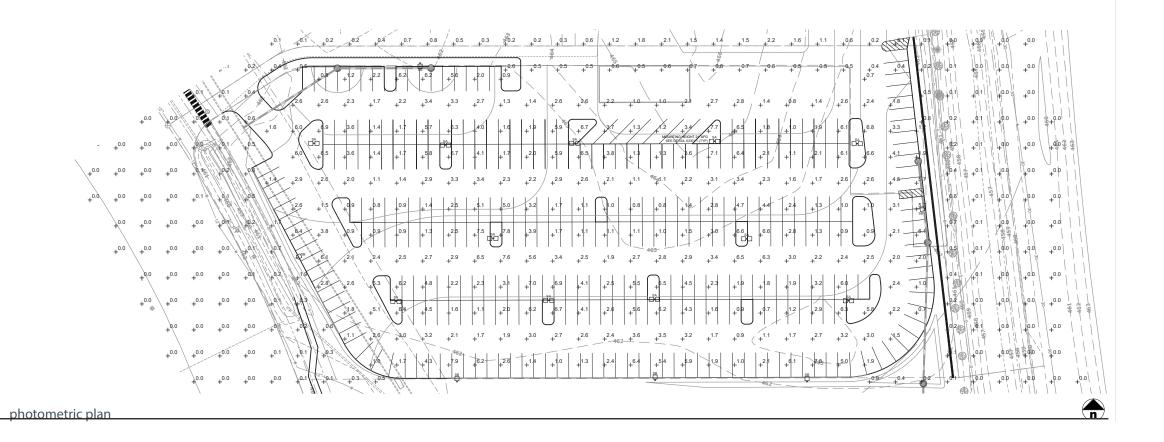
∠EIFS SYSTEM IN

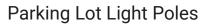
Section 2 @ Screen wall

### Lighting

Lighting fixtures for the site have been selected to serve as a way to provide safety and security on site. Color of light fixtures will match as closely as possible with adjacent sites specifically Lot - 1H where existing exterior light fixtures exist.







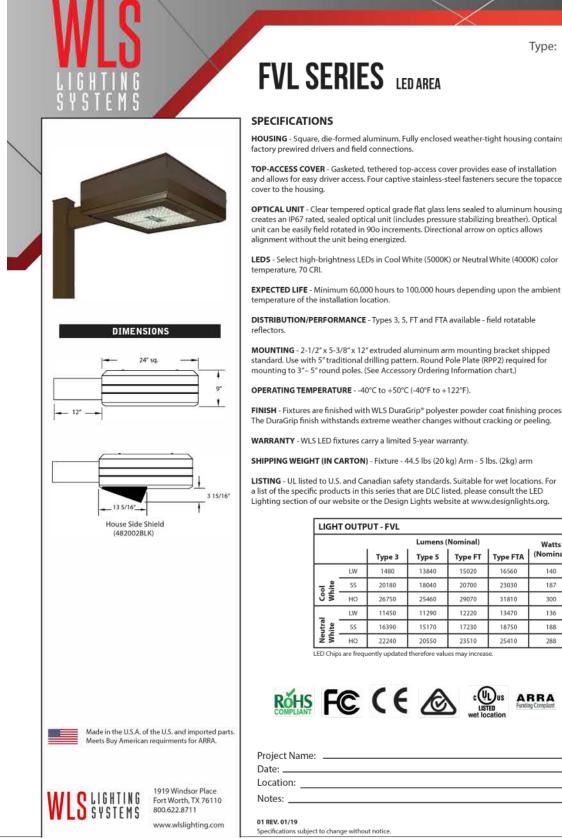
Techlight 20'-0" Steel 4" Square 7 Guage Pole SSP Series Color: Bronze, to match adjacent light poles



### Parking Lot Lights

**WLS Lighting Systems** High Lumen Output **FVL** Series Color: Bronze throughout

**Lighting Cutsheets** 



FVL SERIES LED AREA

#### **SPECIFICATIONS**

HOUSING - Square, die-formed aluminum. Fully enclosed weather-tight housing contains factory prewired drivers and field connections.

TOP-ACCESS COVER - Gasketed, tethered top-access cover provides ease of installation and allows for easy driver access. Four captive stainless-steel fasteners secure the topaccess

**OPTICAL UNIT** - Clear tempered optical grade flat glass lens sealed to aluminum housing creates an IP67 rated, sealed optical unit (includes pressure stabilizing breather). Optical unit can be easily field rotated in 900 increments. Directional arrow on optics allows alignment without the unit being energized.

LEDS - Select high-brightness LEDs in Cool White (5000K) or Neutral White (4000K) color temperature, 70 CRI.

temperature of the installation location.

DISTRIBUTION/PERFORMANCE - Types 3, 5, FT and FTA available - field rotatable

 $\label{eq:mounting} \textbf{MOUNTING} - 2-1/2" \times 5-3/8" \times 12" \text{ extruded aluminum arm mounting bracket shipped standard. Use with 5" traditional drilling pattern. Round Pole Plate (RPP2) required for$ mounting to 3"-5" round poles. (See Accessory Ordering Information chart.)

**OPERATING TEMPERATURE** -  $-40^{\circ}$ C to  $+50^{\circ}$ C ( $-40^{\circ}$ F to  $+122^{\circ}$ F).

 $\textbf{FINISH} - Fixtures \ are \ finished \ with \ WLS \ Dura Grip^* \ polyester \ powder \ coat \ finishing \ process.$ The DuraGrip finish withstands extreme weather changes without cracking or peeling.

WARRANTY - WLS LED fixtures carry a limited 5-year warranty.

SHIPPING WEIGHT (IN CARTON) - Fixture - 44.5 lbs (20 kg) Arm - 5 lbs. (2kg) arm

LISTING - UL listed to U.S. and Canadian safety standards. Suitable for wet locations. For a list of the specific products in this series that are DLC listed, please consult the LED Lighting section of our website or the Design Lights website at www.designlights.org.

LIGHT OUTPUT - FVL								
	Watts							
		Type 3 Type 5 Type FT Type FTA						
	LW	1480	13840	15020	16560	140		
Cool	SS	20180	18040	20700	23030	187		
°₹°	НО	26750	25460	29070	31810	300		
	LW	11450	11290	12220	13470	136		
Neutral White	SS	16390	15170	17230	18750	188		
N X	НО	22240	20550	23510	25410	288		

LED Chips are frequently updated therefore values may increase.



Location:

01 REV. 01/19











### FVL SERIES LED AREA

SERIES	DISTRIBUTION	LIGHT SOURCE	DRIVE CURRENT	COLOR TEMPERATURE	LINE VOLTAGE	LUMINAIRE FINISH	OPTIONS	ACCESSORIES
FVL <sup>1</sup>	FT - Forward Throw FTA - Forward Throw Automotive 3 - Type III 5 - Type V	LED	LW - Low Watt SS - Super Saver HO - High Output	CW - Cool White (5000K) NW - Neutral White (4000K)	UE - Universal Voltage (120-277) 347-480	BLK - Black BRZ - Bronze GPT - Graphite MSV - Metallic Silver PLP - Platinum Plus SVG - Satin Verde Green WHT - White Optional Color Decals 45 - Light Gold 20 - Charcoal Metallic 55 - Black 94 - Blue Metallic 59 - Dark Green 51 - Dark Red 21 - Tomato Red 50 - White 700 - Aztec Silver Metallic	BBK 8" Bracket (5 and D180 only) PC Button-Type Photocell TB Terminal Block SF <sup>2</sup> Single Fusing DF <sup>2</sup> Double Fusing NO No Options	Sensor IMS - Integral Motion Sensor <sup>3</sup> PCI120 - 120V Button-Type Photocell PCI208 - 208V Button-Type Photocell PCI240 - 240V Button-Type Photocell PCI277 - 277V Button-Type Photocell PCI347 - 347V Button-Type Photocell Options 8BK - 8" Bracket (S and D180 only) TB - Terminal Block NA - No Accessories
ORDER	<b>R:</b>							
WLS-FVL		LED						

Example: WLS-FVL-5--LED-SS-CW-UE-BRZ-NO-NA

LUMINAIRE EPA CHART<sup>2</sup> - FVL

D90

TN120

Q90

- Single

**■** ■ D180

8" Bracket

#### **FOOTNOTES:**

Use with 5" traditional drilling pattern. Fusing must be located in the hand hole of pole.

12" Bracket

4.7

7.3

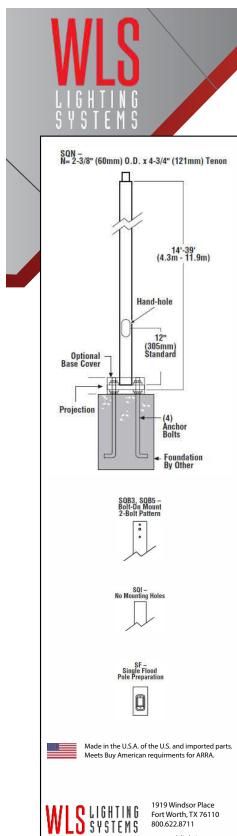
Not compatible with wireless controls system, DIM or BLS option

(339 mm)	$\overline{}$
House Side Shield (4	82002BLK)
	4-16
	( <del>0)</del>

FVL - Type 3

<b>Drive Current</b>	Color Temp.*	Lumens	Watts	LER	<b>BUG Rating</b>
	CW	26,754	300	89	B3-U0-G3
но	CW-HSS	17,316	288	60	B2-U0-G2
	NW	22,240	289	77	B3-U0-G2
	NW-HSS	13,941	288	48	B2-U0-G4
LW	CW	14,082	140	100	B2-U0-G2
	CW-HSS	9205	137	67	B1-U0-G2
	NW	11,451	137	84	B2-U0-G2
	NW-HSS	7513	136	55	B1-U0-G2
	cw	20,181	187	108	B3-U0-G2
**	CW-HSS	13,034	187	70	B2-U0-G2
SS	NW	16,394	189	87	B3-U0-G2
	NW-HSS	10,384	189	55	B2-U0-G2

**Lighting Cutsheets** 



**SNTS SERIES** 

### **SPECIFICATIONS**

Pole shaft is electro-welded ASTM-A500 Grade C steel tubing with a minimum yield strength of 50,000 psi. On Tenon Mount steel poles, tenon is 2-3/8" O.D. high-strength pipe. Tenon is 4-3/4" in length. Straight poles are 4", 5", and 6" square.

Standard hand-hole location is 12" above pole base. Poles 22' and above have a 3" x 6" reinforced hand-hole. Shorter poles have a 2"x 4" non-reinforced hand-hole. BASE - Pole base is ASTM-A36 hot-rolled steel plate with a minimum yield strength of 36,000 psi. Two-piece square base cover is optional.

#### ANCHOR BOLTS

Poles are furnished with anchor bolts featuring zinc-plated double nuts and washers. Galvanized anchor bolts are optional. Anchor bolts conform to ASTM F 1554-07a Grade 55 with a minimum yield strength of 55,000 psi.

#### GROUND LUG

Ground lug is standard.

#### DUPLEX RECEPTACLE

Weatherproof duplex receptacle is optional.

#### GROUND FAULT CIRCUIT INTERRUPTER

Self-testing ground fault circuit interrupter is optional.

Each pole is finished with DuraGrip®, WLS's baked-on polyester-powder finishing process which electrostatically applies and fuses a polyester powder to the pole. Provides an extremely smooth and uniform finish to withstand extreme weather changes without cracking or peeling, and features a five-year limited warranty. Optional DuraGrip® Plus features the added protection of a 3.0 to 5.0 mil thickness of polyesterpowder finish plus an inner coating, as well as a seven-year limited warranty.

#### DETERMINING THE LUMINAIRE/POLE COMBINATION FOR YOUR APPLICATION:

- Select luminaire from luminaire ordering information
- Select bracket configuration if required
   Determine EPA value from luminaire/bracket EPA chart
- Select MPH to match wind speed in the application area (See windspeed maps).
- Confirm pole EPA equal to or exceeding value from note above Consult factory for special wind load requirements and banner brackets

SHIPPING WEIGHTS - SNTS Series	
4"(102mm) sq. 11 Ga. is approximately	7.50 lbs./ft
4"(102mm) sq. 07 Ga. is approximately	10.00 lbs./ft
5"(127mm) sq. 11 Ga. is approximately	9.00 lbs./ft
5"(127mm) sq. 07 Ga. is approximately	12.50 lbs./ft
6"(152mm) sq. 07 Ga. is approximately	15.40 lbs./ft
Anchor Bolts (3/4" x 30")(19mm x 762mm)	15 lbs. (7kg)/se
Anchor Polts (1" v 26")(25mm v 014mm)	30 lbs (14kg)/se

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ate:			
cation:			
otes:			
J.C.J			

### Specifications subject to change without notice

### **SNTS SERIES**

Bolt-on Arm Mount See pole selection guide for patterns and fixture matches.  4\$QB3 3" Reduced drilling pattern  5\$QB3 3" Reduced drilling pattern  4\$QB5 5" Traditional drilling pattern  5\$QB5 5" Traditional drilling pattern  5\$QB5 5" Traditional drilling pattern  4\$QB0  5\$QB0  6\$QB0	<b>511G</b> 11 Ga. Steel <b>507G</b> 07 Ga. Steel	14 14' 16 16' 18 18' 20 20' 22 22' 24 24' 26 26' 28 28' 30 30' 35 35'	S Single/Parallel D180° Double D90° Double DN90° Double T90° Triple TN120° Triple Q90° Quad QN90° Quad	BRZ Bronze BLK Black PLP Platinum Plus WHT White SVG Satin Verde Green GPT Graphite MSV Metallic Silver	GA Galvanized Anchor Bolts SF <sup>2</sup> Single Flood DF <sup>2</sup> Double Flood DGP DuraGrip* Plus LAB Less Anchor Bolts OSXX <sup>4</sup> Pole preparation for PMO: Occupancy Sensor
Tenon Mount See selection guide for tenon and fixture/bracket matches. 4SQN 5SQN 6SQN No Mounting Holes Use with:  • BKA 4ISF & BKA XSISF  Internal Slip-fitter <sup>1</sup> 4SQI 5SQI		<b>39</b> 39'	N Tenon Mount (Standard tenon size is 2-3/8" O.D.)		

Example: WLS-SNTS-4SQB3-S11G-20-T90-BRZ-GA

FOOTNOTES:				
See Area Lighting Brackets 3" Reduced Drill Pattern and Area Lighting Brackets - Bolt-on 5" Traditional Drill Pattern spec sheets for Internal Silp-fitter brackets.  Pole heights will have +/- 1/2" tolerance.  See Flood Lighting Brackets section for choice of FBO brackets.	Order PMOS separately. Change "XX" to indicate height and side of pole location for pole preparation. EX: 0518A indicates preparation is to be 18ft. up from pole base on side A. Optimal distance from ground to sensor is 20ft.      OSXX option required. Not for use with Metal Halide fixtures.			

ACCESSOR ORDERING INFORMATION (Accessorie are field installed)	
Description	Order Numbe
4BC – 4" Square Base Cover	122559CLR
5BC – 5" Square Base Cover	122561CLR
6BC – 6" Square Base Cover	122563CLR
ER2 – Weatherproof Duplex Receptacle	122566CLR
GFI – Ground Fault Circuit Interrupter	122567CLR
MHP – Mounting Hole Plugs (3 plugs)-for use with 5" traditional drill pattern	132336
MHD – Mounting Hole Weatherproof Decal-for use with 3" reduced drill pattern	340120
Vibration Damper - 4" Square Pole (bolt-on mount only)	172539
Vibration Damper - 5" Square Pole (bolt-on mount only)	172538
Vibration Damper - 6" Square Pole (bolt-on mount only)	178361
PMOS120 - 120V Occupancy Sensor	518030CLR5
PMOS208/240 - 208, 240V Occupancy Sensor	534239CLR <sup>5</sup>
PMOS277 - 277V Occupancy Sensor	518029CLR <sup>5</sup>
PMOS480 - 480V Occupancy Sensor	534240CLR <sup>5</sup>



Fort Worth, TX 76110

Project Name:	
Date:	
Location:	
Notes:	

01 REV. 01/19

### **Lighting Cutsheets**

### **EMERGI-LITE**



YPE:			
ATALOG #: _			
OTES:			

### **Lux-Ray™ LED Series**

Die-Cast aluminum LED Emergency Lighting
Interior or Exterior Capable Nickel-Metal
Hydride battery

NEMA-3R

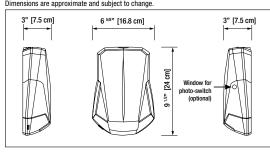
#### **Standard Features**

- Die-Cast aluminum housing, available in four finishes: dark bronze, off-white, black, and platinum gray
- Nema-3R rated for indoor/outdoors wet and damp locations: 0-40°C (32-104°F)
- Wall-mount installation on various junction boxes or via rigid conduit
- Patent-pending design for easy installation: wall-mount back-plate includes electrical wire box with snap-on connector
- Clear polycarbonate lens of reduced size (3" x 1.5"), shock-absorbent and UV-resistant
- Battery: environmentally friendly high-temperature rated, Nickel-Metal Hydride technology
- Power consumption in stand-by: less than 5W
- Optional: Self-test and diagnostic functions, operated by micro-controller
- UL listed to the UL924 standard for wet and Damp locations
- 5-year limited warranty

### **Options**

- Cold-weather: -40°F/-40°C
- Forward-throw light distribution, for applications of outdoor exit discharge (OSHA 1910.36)
- High-lumen output: 25 to 50% additional level of illumination
- Dual-mode operation: normal lighting and/or emergency lighting with separate AC inputs
- Photo-switch: dusk-to-dawn control of normal lighting
- Remote test: infrared remote control (keyboard ordered separately)
- Time delay: 5, 10, 15 minutes

### **Outline and Dimensions**



### **Suggested Specification**

Supply and install the Lux-Ray™ LED Series of emergency lighting unit from EMERGI-LITE®.

The unit body shall include a back-plate and housing made of Die-Cast Aluminum with paint Finish color: \_\_\_\_\_ and a UV- and impact-resistant polycarbonate lens of reduced size: 3" X 1.5". The back-plate shall have knockouts for wires and wall-mount installation box as well as a threaded hole for rigid conduit entry at the top of the unit. The back-plate shall have a built-in electrical box with wire terminals and snap-on connector. After complete electrical installation of the back-plate the equipment housing shall be installed by a simple push & snap over the back-plate.

The emergency lights shall be 4 Power-Light-Emitting Diodes (LED) with operational life of minimum 36,000 hours, until 70% of the initial light level (reported L70). The LED lamps shall have redundant interconnections: eventual failure of one lamp shall allow other LED lamps to function. The unit shall have a dual-voltage input rated: 120/277VAC, 60Hz. The battery charger shall include low voltage disconnect to prevent deep discharge, battery lockout to prevent battery drain prior to energizing the utility power, and brownout protection which will automatically switch unit into emergency mode if the utility power falls below 80% of nominal level.

The unit with Nickel-Metal Hydride battery shall be equipped with a micro-controller-based non-audible Advanced Diagnostic circuit. The unit shall self-test for one minute every month, 30 minutes every six months and 90 minutes annually. The pilot light shall be integrated with the test button; it shall be a bi-color LED and shall change color from normal green to flashing red when a failure is detected from the battery, charger circuit or lamps. A label located inside the unit near the test button shall describe the diagnostic for each flashing code.

When specified, models with dual-mode illumination shall include two separate AC input circuits: for emergency lighting and for normal lighting. When specified, models equipped with photo-switch shall automatically activate the lights only from dusk till dawn, for additional energy savings. The typical ambient illumination for the photo-switch shall be: one foot-candle (to turn-on) and three foot-candles (to turn-off). The unit shall be UL listed to the UL924 Standard for wet and damp locations.

The unit shall be EMERGI-LITE® model:

### **Power Consumption Chart**

AC Specs: 120/277VAC					
Model Type	Normal lighting		Emergency lighting		6-12VDC remote
	Current (max)	Power (max)	Current (max)	Power (max)	Power (max)
AC, 2AC, ACDC, DC	0.12/0.08A	12W	0.11/0.08A	12W	8W
AC, 2AC, ACDC, DC, -H	0.18/0.11A	18W	0.18/0.11A	18W	14W
ACSD, SD, SD-H	0.12/0.06A	12W	0.05/0.02A	5W	Nickel-Metal Hydride battery
SD-CW	-	-	0.15/0.07A	16W	
ACSD-CW, -CWRC	n/	r*	0.22/0.10A	24W	

\*Note: Only unswitched AC input; normal lighting with photo-switch or remote control

#### **Photometry Performance**

Whether installed indoors or outdoors, with spacing measurements for a single unit or between two units center-to-center, the **Lux-Ray<sup>TM</sup> LED** Series delivers a stable and consistent illumination making it easy to specify in a wide range of applications. The outstanding spacing of illumination ranges from 50 to 70 feet for standard units (wide beam) and from 40 to 50 feet with the forward-throw beam option.

#### Average of 1 foot-candle

Table A: Spacing for NFPA101 (average = 1fc, see Note)				
Model Type	Mouting Height	Width X Lenght (Ft)		
		Single Unit	Center-To-Center	
Standard	9'	6' X 50'	6' X 50'	
With option -H	11'	6' X 60'	6' X 60'	
with option -n	11		3' X 70'	
With option -FT	12'	6' X 40'	_	
With option -FTH	15'	6' X 50'	_	

### Indoor reflectance: 80/50/20 and 10-ft wide corridor. Outdoor reflectance: 0/30/10 Note: The illumination level meets ALL the requirements of the Life Safety Code (NEPA 101):

ion level meets ALL the requirements of the Life Safety Code (NFPA 101):

1) Average of 1 foot-candle or more

2) Minimum at any point of 0.1 foot-candle or more

3) Maximum-to-minimum illumination uniformity ratio of 40:1 or less

#### Minimum of 1 foot-candle

Table B: Spacing for minimum illumination= 1fc				
Model type	Mouting Width X lenght (Ft)		( lenght (Ft)	
	Height	Single Unit	Center-to-Center	
Standard	9'	4' x 28'	4' x 32'	
With option -H	11'	4' x 32'	4' x 40'	
With option -FT	12'	4' x 22'	_	
With option -FTH	15'	4' x 27'	_	

Max./min. uniformity ratio less than 3:1

# 50° 50° 6°



#### **How to Order**

#### **Battery Unit**

Color	Series	Model	Options
B= Black	LUX= Lux-Ray LED	SD= Self-Powered & diagnostic	-CW= Cold weather (-4030°C; N/A with option -H)
<b>BZ</b> = Dark bronze		(0 50°C)	-D1= Time delay: 5 minutes, -D2= Time delay: 10 minutes, -D3= Time delay: 15 minutes
<b>OW</b> = Off-white		ACSD= Dual-mode AC /	-FT= Forward throw lighting
PG= Platinum gray		Self-Powered (0 40°C)	-H= High lumen output (max. 30°C; model SD only)
			-P= Photo-switch, normal lighting (ACSD only)
			-RC= Remote control - infrared*
			* Remote control keypad (TB-RC1-E) ordered separately

EXAMPLE: BZLUXACSD-RC

#### Remote Fixture

Homoto Fixturo				
Color	Series	Model	Options	
B= Black BZ= Dark bronze OW= Off-white	LUX= Lux-Ray LED	AC= AC-only ACDC= AC/6-12VDC remote DC= 6-12VDC remote fixture	-FT= Forward throw lighting -H= High lumen output (-4030°C) -P= Photo-switch, normal lighting (AC, ACDC only)	
<b>PG</b> = Platinum gray		<b>2AC</b> = AC-only two circuits: 120/120 or 277/277V	-RC= Remote control - infrared* (AC, ACDC only)  * Remote control keypad (TB-RC1-E) ordered separately	

EXAMPLE: BZLUXDC-FTH

#### Thomas@Betts

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## **Proposed Site - Materials**

#### **Material & Finish Selections**

Materials & finishes for this site have been selected to coherently exist with adjacent properties to acheive Prairie Style of Architecture. The goal is to tastefully align the proposed site with EchoPark standard for an inviting and engaging environment, while blending in with the overall development design theme.

### **Exterior Paint**



**Description: Exterior Paint** Manufacturer: Sherwin Williams Color: Univeral Khaki - SW6150 Finish: Eggshell Location: Bollards and bollard rails, Car wash wall cap. **Contact: Sherwin Williams** 

## Textured Coating (Stucco)



**Description: Stucco Textured** Coating Manufacturer: Sherwin Williams Color: Universal Khaki - SW6150 Location: Screen wall and car wash building

## Carwash Overhead Door



**RAL 1013** 

Description: Car wash overhead door color Color: Ral 1013 Location: Car wash overhead door

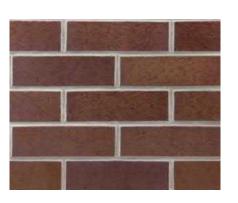


**Description: Cookson Rollup Security Doors** Manufacturer: Cookson Model: ESD10

Color: Ral 1013 Location: Car wash building

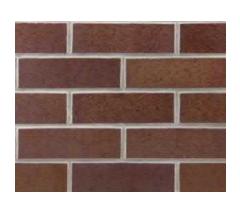
## Masonry Veneer

(800) 474-3794



Description: Brick veneer Manufacturer: Endocott Color: Medium Ironspot #77 Location: Carwash watertable

## Brick (EIFS)



Description: EIFS Brick texture

Finish

Manufacturer: TBD

Color: match Endicott Medium

Ironspot 77

Location: Screen wall piers

## **Appendix - Reference Photos**

Bollards and Security Gates at Other Echo Parks



Non-Illuminated Security Gate at Echo Park Store



Bollard Rail Implemented at Echo Park Store

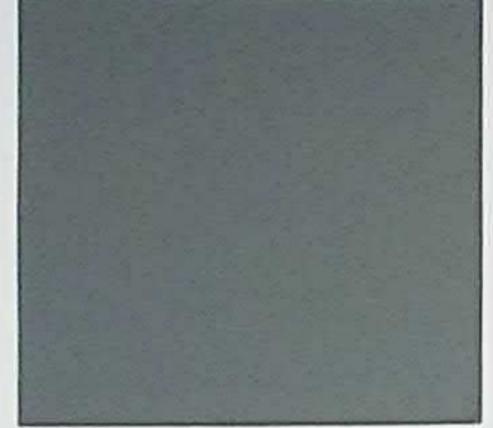
# Material Samples

EchoPark St. Louis, MO - Lot 1H - Material Board

## Exterior Materials & Finishes



Stucco Medium Coarse Texture Sherwin Williams 6150 Universal Khaki



Paint Sherwin Williams 7067 Cityscape



Benjamin Moore 427 Napa Vineyards



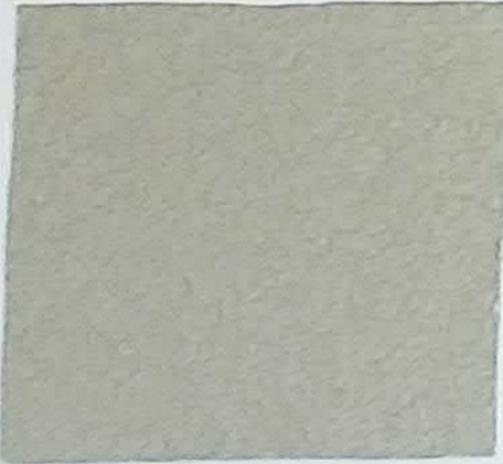
Standing Seam Roof and Bike Rack Finish Kingspan

Silversmith Mica

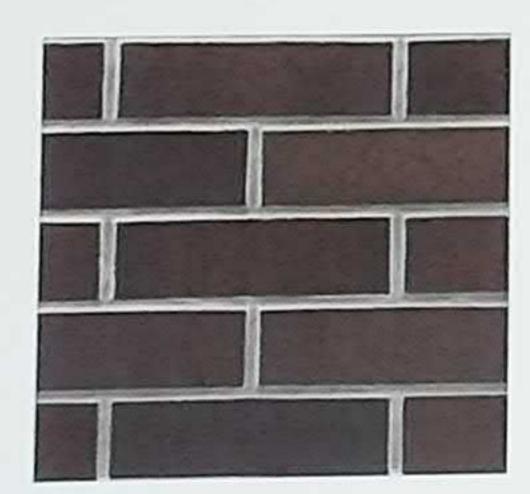
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EchoPark - St. Louis, MO - Lot 1G - Material Board

## Exterior Materials & Finishes



Stucco Medium Coarse Texture Sherwin Williams 6150 Universal Khaki



Brick Veneer Endicott Medium Ironspot 77 See included sample board



Brick (EIFS) Match to Endicott Medium Ironspot 77



Garage Door Clear Polycarbonate



wd WDPARTNERS.COM 1

ENDICOTT THIN BRICK & TILE LLC 402-729-3323 WWW.ENDICOTT.COM MEDIUM IRONSPOT #77

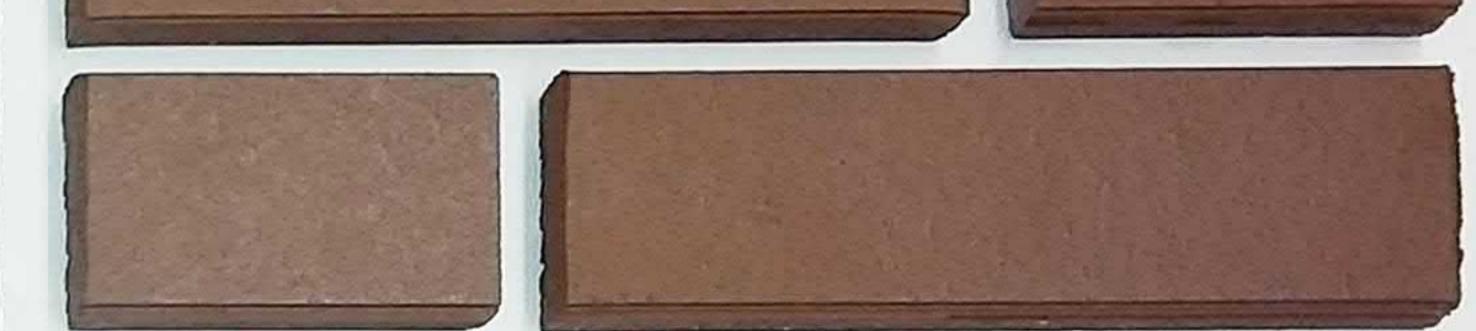
SMOOTH

City of Chesterfield 08/06/2021

RECEIVED

Department of Public Services













## **DRYVIT CUSTOM BRICK™**

The Classic Look of Brick, from an Environmentally-Superior, Cost-Effective, Lightweight Alternative





Custom Brick<sup>™</sup> is an exciting alternative from Dryvit<sup>®</sup>, the nation's leading manufacturer of exterior insulation and finish systems. This dramatic finish provides a beautiful effect that will leave even the most experienced mason shaking his head in disbelief. The look of Custom Brick is simply amazing, and it has numerous advantages over other exterior cladding options.

Custom Brick is a textured finish applied using a template to create a stunning finish. It is available in a wide variety of patterns, colors, textures and shading techniques that can be used to make your building look exactly the way you want it to. No more settling for the design limitations of other exteriors — or their excessive price — in getting exactly the look you want.

FEATURE	BENEFIT
Realistic brick masonry aesthetic	Inexpensively emulates labor-intensive masonry
Lightweight	Reduces structural demands for exterior facade
14 standard templates	Ready supply of most popular designs
<ul> <li>Custom templates available</li> </ul>	Ability to match most existing patterns and textures
<ul> <li>Multiple thicknesses of templates</li> </ul>	Ability to create and shape deeper grout lines
<ul><li>Vapor-permeable</li></ul>	Will not trap moisture vapor
■ 100% acrylic	Greater flexibility for crack resistance and longer life
<ul> <li>Exterior or interior application</li> </ul>	Adds natural texture and color to multiple surfaces
■ DPR & PMR chemistry	Resistant to dirt and the growth of mold and mildew



Brown Street Station Condos | Forest Park, IL

"We wanted an antique brick look that you just couldn't get in this area without paying a fortune and waiting forever to get it. Custom Brick provided a perfect — and accessible — solution."



Marriott Residence Inn | Salt Lake City, UT

Custom Brick saved the owner over \$200,000 by eliminating the heavier footings and framing needed for the clay brick. "We are extremely pleased with the appearance of the finish."

John Ailport, General Superintendent, Silver Moon Properties

Woodburn Associates. Owner

## A TEMPLATED BRICK APPEARANCE FOR ANY WALL.

#### Value:

Low installed cost, and a 30-year lifecycle

### Lightweight:

Approximately 1/35<sup>th</sup> the weight of clay brick

#### **Design Freedom:**

Ultimate flexibility with color, texture and shapes

A Dryvit® Outsulation® system with Custom Brick™ finish provides an environmentally superior, cost-effective alternative, all from a single source.

Now that's a better brick!

### **INSTALLATION OVERVIEW:**

- 1. Prepare surface
- 2. Apply grout coat
- 3. Adhere selected Custom Brick template
- 4. Mix, then apply selected Custom Brick Finish material evenly
- 5. Create custom shading and effects (optional)
- 6. Remove templates and touch-up, if needed
- 7. Allow to dry



Isringhausen Imports | Springfield, IL

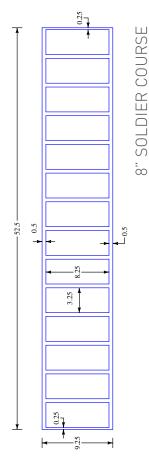
The owner liked the renovated showroom so much he decided to re-clad his other brick building with Custom Brick because it looked better than the original brick.

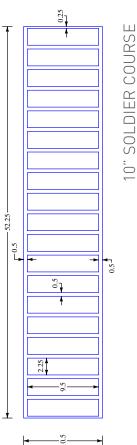


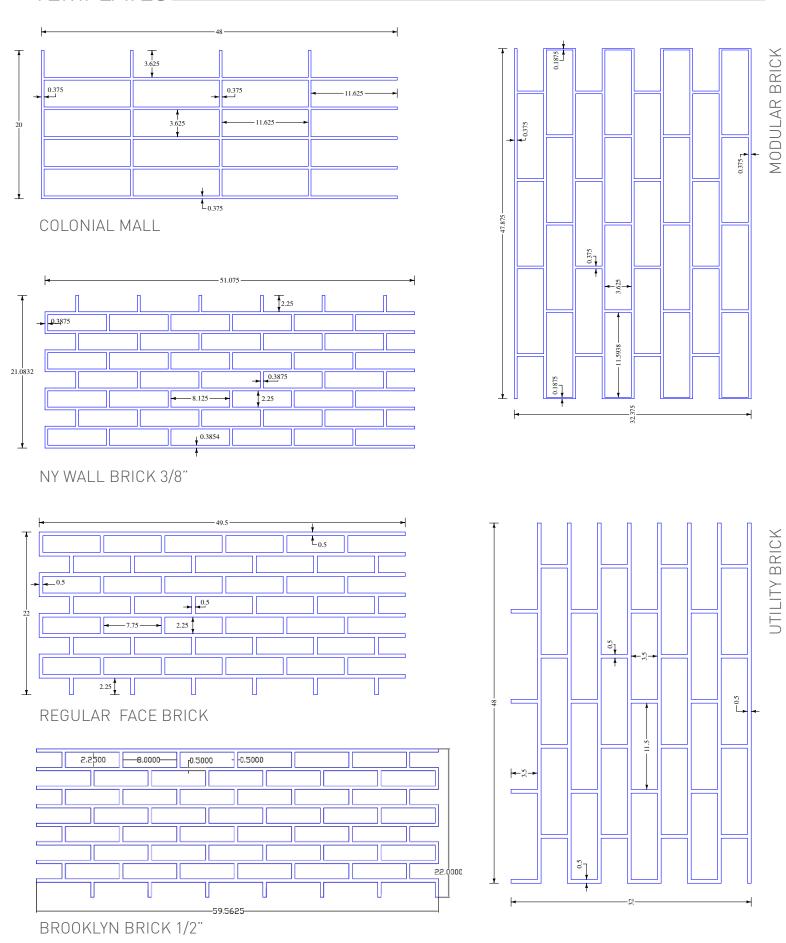


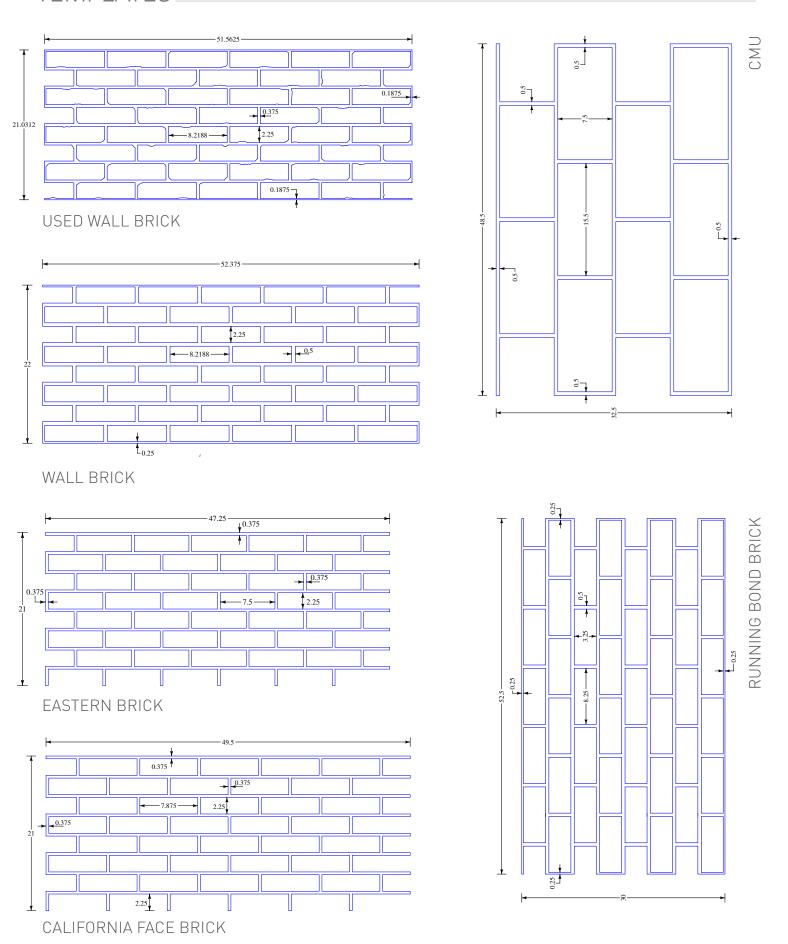


## **TEMPLATES**









## PATTERN DETAILS

Pattern Name	Total Size	Avail. Depths	Ft² / piece	Ft <sup>2</sup> / box	Pcs./box
Colonial Mall	20" x 48"	24pt / 60pt	6.67	400	60
NY Wall Brick 3/8"	21.08" x 51.08"	24pt / 60pt	7.48	403	54
Utility Brick	32" x 48"	24pt / 60pt	10.67	405	38
Running Bond	29.5" x 52"	24pt / 60pt	10.94	404	37
8" Soldier Course	9.25" x 52"	24pt / 60pt	3.37	404	120
10" Soldier Course	10.375" x 52"	24pt / 60pt	3.81	400	105
Wall Brick	22" x 53"	24pt / 60pt	8.00	400	50
Used Wall Brick	21" x 52"	24pt / 60pt	7.52	406	54
Modular Brick	32.5" x 48"	24pt / 60pt	10.76	408	38
CMU	32.5" x 48.5"	24pt / 60pt	10.95	405	37
Eastern Brick	21.5" x 48.25"	24pt / 60pt	7.20	403	56
California Face Brick	21" x 49.5"	24pt / 60pt	7.22	404	56
Regular Face Brick	22" x 49.5"	24pt / 60pt	7.56	400	53
Brooklyn Brick 1/2"	22" x 59.5625"	24pt / 60pt	9.10	450	50



Second National Bank | Arcanum, OH

## VERSATILE DESIGN CAPABILITY FOR ANY PROJECT

Custom Brick is available in 14 standard brick designs with a wide array of standard colors. Custom colors are also available to tailor to your project's specifications, and custom patterns can be made. Custom template options are quoted separately and minimum order quantities apply, with slightly longer lead times. Contact your Dryvit Representative or distributor for a quote.

Examples of standard templates:



Used Wall Brick



NY Wall Brick 3/8"



Utility Brick



Regular Face Brick



Germantown, LLC | Nashville, TN



Vanderbilt University | Nashville, TN

Dryvit Systems, Inc.  $\,$  I One Energy Way  $\,$  I West Warwick, RI, 02893  $\,$  I 401.822.4100  $\,$  I dryvit.com

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# THANK YOU