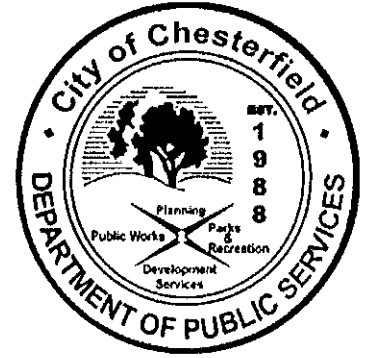


**III.A.**



**DATE:** February 13, 2013

**TO:** Michael G. Herring, ICMA-CM  
City Administrator

**FROM:** James A. Eckrich, P.E.  
Public Works Director / City Engineer

**RE:** Schoettler Road Bridge TIP Application

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As part of our capital improvement planning, City Staff makes every effort to utilize available grant monies. One such grant source is the Transportation Improvement Program (TIP) administered by East West Gateway. The TIP funds up to eighty percent of construction, right of way, and design costs associated with infrastructure improvements to eligible roads and bridges. TIP funds have been successfully obtained in the recent past to fund projects on Appalachian Trail, Edison Road, Ladue Road, Greentrails Drive, and Timberlake Manor.

The City of Chesterfield has recently funded a study of Schoettler Road in order to plan for future improvements on that roadway. Any improvements will necessitate the replacement of the Schoettler Road Bridge, located just north of Greenleaf Valley Drive, shown on the attached map. This bridge is undersized and has deteriorated to the point that it qualifies for replacement under the On-System Bridge Program (BRM) of the TIP. Accordingly, Staff is in the process of drafting a TIP application to East West Gateway for the replacement of this bridge. If successful, the project would be designed in 2015, right of way acquisition would occur in 2016, and the project would be constructed in 2017.

The TIP application requires an application fee of 0.5% of the federal (grant) portion of the project, in this case \$8,012.40. If the grant is not approved the entire application fee will be refunded to the City. While there are never any guarantees, City Staff is cautiously optimistic that this grant will be approved.

#### **Action Recommended**

This matter should be forwarded to the Planning and Public Works Committee for its consideration of approval of the submittal of a grant application for the replacement of the Schoettler Road Bridge and the associated application fee of \$8,012.40. Funding for the grant application would be provided from the Capital Projects Fund.

*ohid*  
*JGH*  
*2/13/13*

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# MEMORANDUM

**To:** Jim Eckrich, PWD/CE  
**From:** Kim Streicher, Civil Engineer *KS*  
**Date:** February 11, 2013  
**Re:** EWGateway Transportation Improvement Program Application

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EWGateway recently announced that it is accepting applications for Surface Transportation Program - Suballocated (STP-S), Congestion Management and Air Quality Improvement (CMAQ), and On-System Bridge Program (BRM) projects. After reviewing potential projects in the City, staff recommends applying for the replacement of the Schoettler Road Bridge over Creve Coeur Creek.

This bridge has just recently become eligible for federal funding. The main deficiency is the bridge width, which cannot accommodate even two full twelve foot lanes, and there are no shoulders. The bridge also has a history of requiring extensive maintenance for scour, and is exhibiting advanced deterioration of the curbs.

City staff recommends designing a bridge which would be wide enough to meet all present geometrical standards, long enough to avoid any rise in flood elevations, and would include bike/pedestrian facilities on each side of the bridge to accommodate Chesterfield's Bikeable Walkable Community Plan.

For this round of applications there is money available for fiscal years 2015, 2016, and 2017. The schedule for the project would be to design the replacement in FY 2015, acquire right-of-way in FY 2016, and construct in FY 2017.

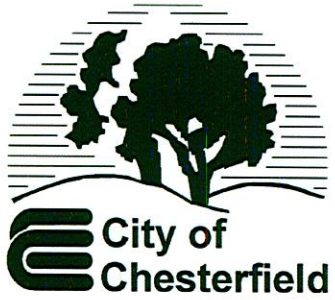
The table below shows the opinion of probable project costs along with the Federal Funds and City 20% match.

Project	Federal Funds	City 20% Match	Total Project Cost
Schoettler Road Bridge Replacement	\$1,602,480	\$400,620	\$2,003,100

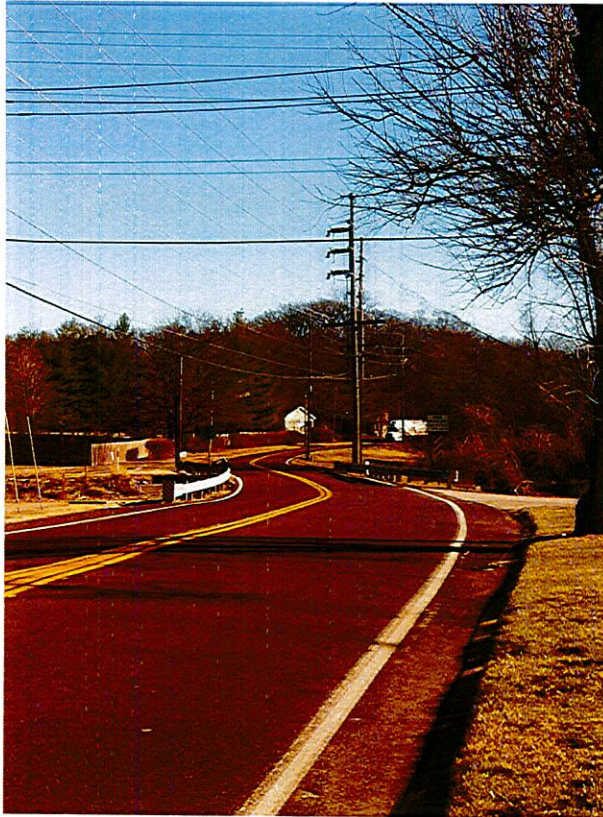
EWGateway applications are due on March 8, 2013 and staff would like your concurrence on submitting an application. Please note that an application fee of ½ of 1% of the federal funds being requested is required. This fee will be refunded if the project is not accepted for federal funding. The STP-S application fee amount is \$8,012.40.

If you have any questions or need additional information please advise.

Cc: Susan Mueller, Principal Engineer

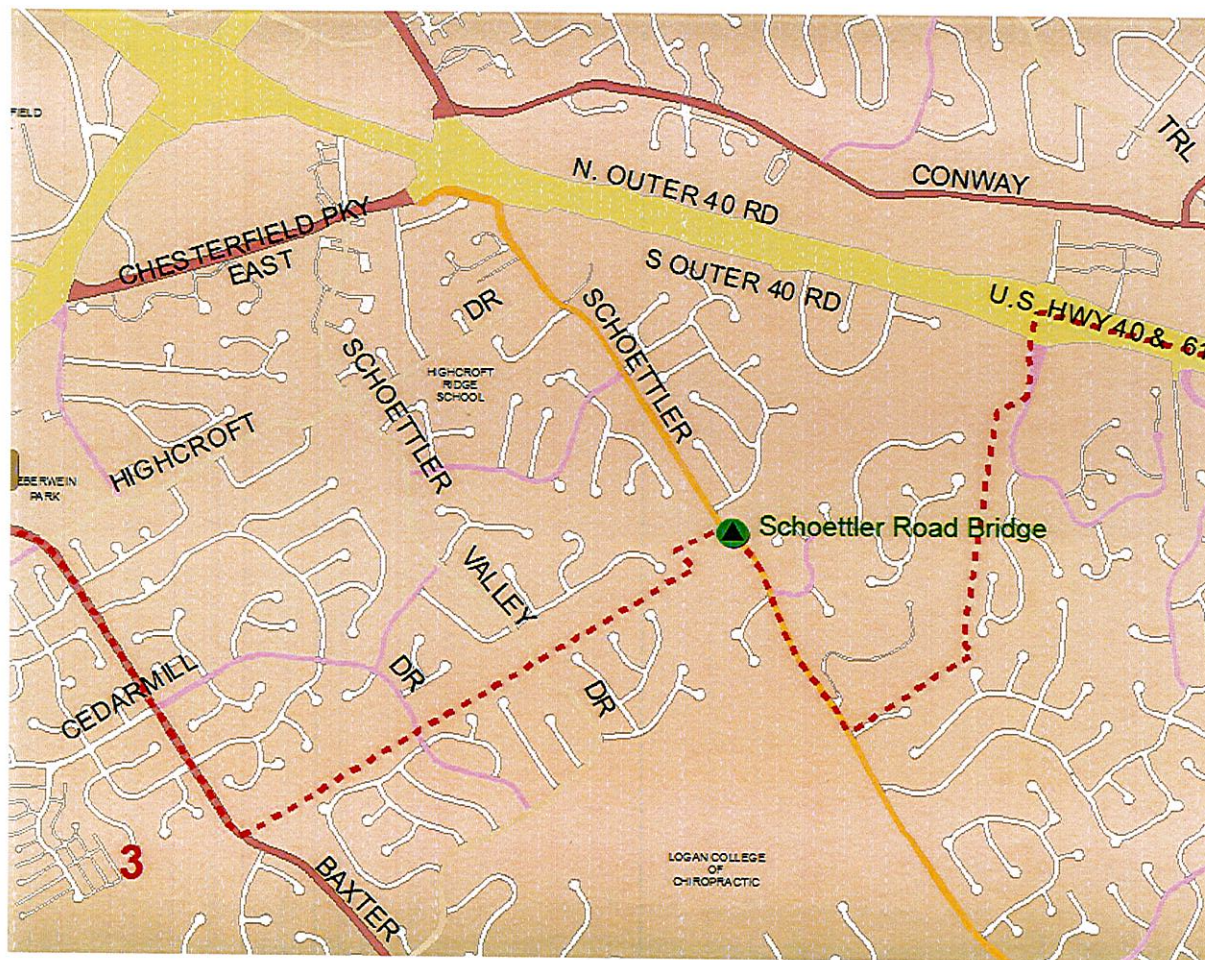


# Schoettler Road Bridge Replacement





# Schoettler Road Bridge Replacement





**Missouri Department of Transportation  
Bridge Inventory and Inspection System  
Non-State Structure Inspection Report**

December 13, 2012  
4:00:22pm

County : ST. LOUIS      Class : NONSTATBR      Design No. : 0793004      Federal ID : 15658

[5D] Route :	00000	[41] Structure Status :	P-POSTLOAD
[4] Place Code :	13600      CHESTERFIE	[9] Location :	S 14 T 45 R 4 E
[6] Features Intersected :	CREVE COEUR CR	[22] Owner :	CITY
[7] Facility Carried :	SCHOETTLER RD	[26] Functional Classification :	UCOLLECT
[16] Latitude :	38 38 26.9 (DMS)	[21] Maintenance Responsibility :	CITY
[17] Longitude :	90 32 26.73 (DMS)		

**AGE AND SERVICE - GEOMETRIC DATA - MATERIAL**

[27] Year Built :	1959	[106] Year Reconstructed :	
[49] Structure Length :	38 FT.	[51] Bridge Width :	23 FT. 2.4 IN.
[32] Approach Roadway Width :	21 FT. 0 IN.	[52] Deck Width :	24 FT. 2.4 IN.

COMPONENTS	# OF SPANS	MATERIAL	CONSTRUCTION
[43] Main series :	1	STEEL	WFGIR
[44] Approach Series :			
[107] Deck Type :		REINCONC	CIP
[108A] Wearing Surface :		ASPHALT	BITUMSEAL
[108B] Membrane :		NOTAPPLIC	NONE
[108C] Deck Protection :		NOTAPPLIC	NONE

**AADT INFORMATION**

[29] ADT on Structure :	7,282	[30] Year :	2010	[109] AADT Truck :	5 %
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**STRUCTURE POSTING**

<b>FIELD POSTING</b>	Problem Code :	Problem Direction Code :
Category :	S-15 TRUCK WEIGHT LIMIT 64 TONS EXCEPT SINGLE UNIT TANDEM REAR AXLE TRUCKS 34 TONS WEIGHT LIMIT	
Ton 1 :	64	Ton 2 :      34      Ton 3 :

<b>APPROVED POSTING</b>	Problem Code :	Problem Direction Code :
Category :	S-15 TRUCK WEIGHT LIMIT 64 TONS EXCEPT SINGLE UNIT TANDEM REAR AXLE TRUCKS 34 TONS WEIGHT LIMIT	
Ton 1 :	64	Ton 2 :      34      Ton 3 :

**STRUCTURE GENERAL INSPECTION**

Inspector	ID No.	Organizational Affiliation
JOSEPH MOLINARO (NON) VINCE KAIMANN	MODOT0622 MODOT0619	MODOT MODOT
[90] Inspection Type	Inspection Date	[91] Frequency
GENERAL	12/27/2011	24

**STRUCTURE OTHER INSPECTION**

Type	Category	Date	Freq	PIN	NBI
UNDERWATER	WADE	12/27/2011	24	N	N



**Missouri Department of Transportation  
Bridge Inventory and Inspection System  
Non-State Structure Inspection Report**

December 13, 2012  
4:00:22pm

County : ST. LOUIS

Class : NONSTATBR

Design No. : 0793004

Federal ID : 15658

**STRUCTURE RATING**

[58] Deck :	5-FAIR CONDITION	5/12/2008
[59] Superstructure ** :	5-FAIR CONDITION	5/12/2008
[60] Substructure ** :	5-FAIR CONDITION	5/12/2008
[61] Channel Protection :	6-WIDESPREAD MINOR DAMAGE	5/4/2004
[62] Culverts **:	N-NOT APPLICABLE	3/1/2002
[36A] Bridge Railing :	0 DOESNT MEET CURRNT STND	4/18/2006
[36B] Transitions Railing :	0 DOESNT MEET CURRNT STND	4/18/2006
[36C] Approach Railing :	0 DOESNT MEET CURRNT STND	4/18/2006
[36D] Rail End Treatment :	1 MEETS CURRENT STANDARDS	4/18/2006
[71] Waterway Adequacy :	SIGNIFICANT DELAY APPRCH	8/12/2002
[72] Approach Roadway Alignment :	6-SATISFACTORY	8/12/2002
[113] Scour Assessment ** :	5-FOUNDATION STABLE	3/1/2002
Type of Scour Evaluation	OBSERVED	
[67] Structure Evaluation :	5-BETTER THAN MINIMUM	3/1/2002
Sufficiency Rating :	49.10 %	3/1/2002
Deficiency :	FUNCTIONAL	3/1/2002
[68] Deck Geometry :	2-BASICALLY INTOLRBLE REQ	3/1/2002
[69] Underclearance :	N-NOT APPLICABLE	3/1/2002

\*\* If RATING lowered to a 3, forward rating info and photos to Bridge Division

**COMMENTS**

<b>General Comments :</b>	A SINGLE SPAN WIDE FLANGE STEEL I-BEAM STRUCTURE ON FULL HEIGHT GRAVITY REINFORCED CONCRETE ABUTMENTS ON SPREAD FOOTINGS. THIS STRUCTURE HAS BEEN TURNED OVER TO THE CITY OF CHESTERFIELD. PREVIOUS BRIDGE NUMBER 096B816
<b>Deck Rating Comments :</b>	TOPSIDE- OBSCURED BY OVERLAY. CURBS ARE SPALLED WITH REBAR ENTIRELY EXPOSED AND RUSTED IN PLACES. UNDERSIDE- MAP CRACKING WITH EFFLORESCENCE. 15% SATURATED
<b>Superstructure Comments :</b>	BEAMS- RUST ON BOTTOM FLANGES ESPECIALLY BELOW GUARDRAIL POST CONNECTIONS ON EXTERIOR BEAM. TOP FLANGES ON ALL GIRDERS ARE RUSTING TO VARYING DEGREE. WORST IS EXTERIOR WEST BEAM WHICH HAS PACK RUST. BEARINGS-HEAVY PACK RUST.
<b>Substructure Comments :</b>	SOME CRACKING OF MORTAR AT WINGWALLS. PREVIOUSLY PATCHED AREAS ARE CRACKING AT SOUTHEAST AND SOUTHWEST CORNERS. A FEW SPALLS BELOW THE ABUTMENT WALL "SEAT" AT SOUTH ABUTMENT. SAME SETTLEMENT BETWEEN NORTHWEST WING AND NORTH ABUTMENT WALL. VOID AT SOUTH EAST CORNER
<b>Channel Protection Comments :</b>	SOME DEBRIS ACCUMULATION. WATER IS DEEPEST AT THE BRIDGE THAN IMMEDIATELY UP OR DOWNSTREAM.
<b>Culvert Comments :</b>	
<b>Bridge Railing Comments :</b>	
<b>Transition Railing Comments :</b>	
<b>Approach Railing Comments :</b>	
<b>Rail End Treatment Comments :</b>	
<b>Water Adequacy Comments :</b>	BRIDGE DECK ABOVE ROADWAY APPROACHES. OCCASIONAL OVERTOPPING OF ROADWAY APPROACHES WITH INSIGNIFICANT TRAFFIC DELAYS.
<b>Approach Roadway Comments :</b>	MINOR REDUCTION IN SAFE OPERATING SPEED REQUIRED DUE TO HORIZONTAL ALIGNMENT.
<b>Scour Assessment Comments :</b>	NO SCOUR.
<b>Work Comments :</b>	MUD JACK VOID

Design\_No = 0793004